



STATE OF DELAWARE  
DEPARTMENT OF TRANSPORTATION  
PO BOX 778  
DOVER, DELAWARE 19903

JACK MARKELL  
GOVERNOR

JENNIFER COHAN  
SECRETARY

**VIA OVERNIGHT DELIVERY**

(302) 760-2030  
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February 11, 2016

Contract No. T200911302.01  
Federal Aid Project No. NH-2015(22)  
US 301 & SR 1 Interchange  
New Castle County

Ladies and Gentlemen:

Enclosed is Addendum No. 2 for the referenced contract consisting of the following:

1. The Bid Proposal Cover, revised, to be substituted for the same page in the Proposal.
2. One (1) page, Special Provision 623514-Post Tensioning Strand System, page 193, has been revised, to be substituted for the same page in the Proposal.
3. One (1) page, Special Provision 708512-Drainage Inlet, Special I and 708513-Drainage Inlet, Special II, page 194, has been revised, to be substituted for the same page in the Proposal.
4. One (1) page, Bid Proposal Form, page 3 revised, to be substituted for the same pages in the proposal. Line Item 3000 has been added.
5. The following Plan Sheets have been revised and replaced: 7, 23, 24, 82, 89, 97, 380, 419, 420, 421, 422, 423, 424, 425 and 426.

Please note the revision listed above and submit your bid based upon this information.

Sincerely,

*signature on file*

James H. Hoagland  
Contract Services Administrator

:jhh  
Enclosure

STATE OF DELAWARE



DEPARTMENT OF TRANSPORTATION

BID PROPOSAL

for

CONTRACT T200911302.01

FEDERAL AID PROJECT NO. NH-2015(22)

US 301 & SR 1 INTERCHANGE

NEW CASTLE COUNTY

ADVERTISEMENT DATE: January 12, 2016

COMPLETION TIME: 782 Calendar Days

SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION  
DELAWARE DEPARTMENT OF TRANSPORTATION  
AUGUST 2001

Bids will be received in the Bidder's Room at the Delaware Department of Transportation's Administration Building, 800 Bay Road, Dover, Delaware until 2:00 P.M. local time **February 16, 2016**

**References.**

- (a) American Association of State Highway and Transportation Officials (AASHTO)
  - (1) LRFD Bridge Design Specifications, 6<sup>th</sup> Edition.
  - (2) LRFD Bridge Construction Specifications, 3<sup>rd</sup> Edition.
- (b) American Society for Testing and Materials (ASTM)
- (c) Post-Tensioning Institute (PTI)
  - (1) Acceptance Standard for Post-Tensioning Systems
  - (2) 6th edition of PTI Post-Tensioning Manual
  - (3) Specification for Grouting of Post-Tensioned Structures
- (d) American Segmental Bridge Institute (ASBI)
- (e) FIB Technical Report, Bulletin 7

**Quality Assurance.**

- (a) **Installer Qualifications.** Engage an experienced installer who has completed post-tension concrete work similar in material, design, and extent to that indicated for this project and with a record of successful in-service performance.
- (b) **Fabricator Qualifications.** Engage a firm experienced in producing post-tension hardware similar to those indicated for this project and with a record of successful in-service performance, as well as sufficient production capacity to supply hardware without delaying the work.
- (c) **Professional Engineer Qualifications.** A Professional Engineer registered in the State of Delaware and experienced in providing engineering services of the kind indicated. Engineering services are defined as those performed for design and installation of post-tensioning for cast-in-place concrete girders or pier caps that are similar to that indicated for this project in material, design, and extent.

**Method of Measurement and Basis of Payment:**

The preparation, testing, furnishing, installing, stressing, grouting and anchorage protection of the post-tensioning system in the various concrete units will be paid for at the contract lump sum price for the Post-Tensioning Pier Strands item. The payment will be full compensation for all tendons, anchorages, ducts, grout as well as all material, labor, equipment, tools, and incidentals necessary to complete the work.

The quantity of post-tensioning strand system will be measured by determining the theoretical weight, in pounds, of the post-tensioning strands placed as shown on the Plans and accepted. The quantity of post-tensioning strand system will be paid for at the Contract unit price per pound. Price and payment will constitute full compensation for the preparation, testing, furnishing, installing, stressing, grouting and anchorage protection of the post-tensioning system in the various concrete units, and for all tendons, anchorages, ducts, and grout, as well as all material, labor, equipment, tools, and incidentals necessary to complete the work.

12/21/15 2/10/16

**708512 – DRAINAGE INLET, SPECIAL I**  
**708513 – DRAINAGE INLET, SPECIAL II**  
**708514 – DRAINAGE INLET, SPECIAL III**  
**708515 – DRAINAGE INLET, SPECIAL IV**  
**708516 – DRAINAGE INLET, SPECIAL V**  
**708517 – DRAINAGE INLET, SPECIAL VI**  
**708518 – DRAINAGE INLET, SPECIAL VII**  
**708653 – DRAINAGE INLET, SPECIAL VIII**  
**708654 – DRAINAGE INLET, SPECIAL IX**  
**708655 – DRAINAGE INLET, SPECIAL X**  
**708656 – DRAINAGE INLET, SPECIAL XI**  
**708657 – DRAINAGE INLET, SPECIAL XII**

**Description:**

This work consists of furnishing and placing a reinforced concrete drainage inlet at the locations shown on the Plans.

**Materials:**

Materials shall conform to the requirements of Section 611, 612, 708, 812 and 824 of the Standard Specifications.

**Construction Methods:**

Special inlets shall be placed in conformance with the requirements of Sections 602 and 708 of the Standard Specifications, and with the details, dimensions, and notes as shown in the details found in the Plans and at the location shown on the Plans.

**Method of Measurement:**

The quantity of inlets will be measured as the actual number of each type installed and accepted. ~~No measurement for payment will be made for outlet pipes or pipe fittings. Inlet and outlet pipe will not be measured under this item, but will be measured with the adjoining pipe under the appropriate item for the size and type of pipe installed.~~

**Basis of Payment:**

The quantity of inlets will be measured and paid for at the Contract unit price per each, installed and accepted. Price and payment will constitute full compensation for furnishing, hauling, and installing all materials, including ~~outlet pipes, pipe fittings,~~ concrete and bar reinforcement, any necessary fittings, frames, and grates; for excavating, backfill, backfilling, compacting, roadway patching materials, roadway patching, and disposing of surplus materials; for cribbing, shoring, and sheeting; and for all labor, equipment, tools, and incidentals required to complete the work. If rock is encountered, rock excavation will be paid under Section 206.

~~Inlet and outlet pipe will be paid for under the appropriate item for the size and type of pipe installed.~~

9/16/10 2/10/16



DELAWARE DEPARTMENT OF TRANSPORTATION  
SCHEDULE OF ITEMS

PAGE: 3  
DATE:

CONTRACT ID: T200911302.01 PROJECT(S): NH-2015 (22)

All figures must be typewritten.

CONTRACTOR : \_\_\_\_\_

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0190	618560 INSTALL STEEL PIPE PILES, 16" DIAMETER	678.000 LF				
0200	618561 INSTALL STEEL PIPE TEST PILES, 16" DIAMETER	215.000 LF				
0210	619501 PRODUCTION PILE RESTRIKE	3.000 EACH	500.00000		1500.00	
0220	619502 TEST PILE RESTRIKE	1.000 EACH	1000.00000		1000.00	
0230	619519 DYNAMIC PILE TESTING BY CONTRACTOR	8.000 EACH				
0240	619539 SIGNAL MATCHING ANALYSIS BY CONTRACTOR	8.000 EACH				
0250	623001 PRESTRESSED REINFORCED CONCRETE MEMBERS, I-BEAMS	LUMP	LUMP			
0260	712006 RIPRAP, R-5	5.000 SY				
0270	713003 GEOTEXTILES, RIPRAP	10.000 SY				
3000	202508 WETLAND ACCESS ROAD, TYPE II	LUMP	LUMP			
	SECTION 0001 TOTAL					

SECTION 0002 ROAD

PROJECT NOTES (CONT.)

SECTION 400

- 18. MEASURES FOR MAINTAINING PUBLIC TRAFFIC, SUCH AS TEMPORARY ROADS, DETOURS, RUN-AROUNDS, ETC, SHALL BE CONSTRUCTED UTILIZING THE APPLICABLE STANDARD BID ITEMS, NOT TEMPORARY ROADWAY MATERIAL (TRM). TRM IS INTENDED FOR MAINTAINING INGRESS AND EGRESS TO PROPERTIES OR BUSINESSES AS WELL AS MAINTENANCE OF EXISTING PUBLIC ROADWAYS. TRM SHALL ALSO BE USED TO MAINTAIN DETOUR ROADS, ETC. AFTER THEIR INITIAL CONSTRUCTION.
- 19. PRIOR TO PLACEMENT OF ANY SECTION OF PCC PAVEMENT, THE UNDERLYING BASE COURSES OF SOIL CEMENT AND PERMEABLE TREATED BASE SHALL BE COMPLETED TO THEIR FULL WIDTH (OUTSIDE OF SHOULDER TO OUTSIDE OF SHOULDER) AND THE UNDERDRAIN AND UNDERDRAIN OUTLETS INSTALLED FOR THE ENTIRE SECTION OF PAVING BEING CONSIDERED BY THE CONTRACTOR.
- 20. THE CONTRACTOR SHALL SCHEDULE HIS WORK SO THAT ALL PERMEABLE TREATED BASE (PTB) PLACED DURING ANY ONE CONSTRUCTION SEASON IS COVERED WITH PCC OR WARM MIX PAVEMENT, AS APPLICABLE, BY THE END OF THE CONSTRUCTION SEASON. ANY PTB WHICH HAS NOT BEEN PAVED OVER AT THE END OF THE SEASON MUST BE ENTIRELY COVERED WITH POLYETHYLENE SHEETING, PROPERLY ANCHORED AND OVERLAPPED AT LEAST EIGHTEEN INCHES FOR THE WINTER AND UNTIL PAVING OPERATIONS RESUME. NO CONSTRUCTION TRAFFIC OF ANY KIND WILL BE PERMITTED TO TRAVERSE OVER PTB AT ANY TIME, EITHER UNCOVERED OR COVERED WITH POLYETHYLENE, EXCEPT FOR NECESSARY EQUIPMENT UTILIZED DURING PAVING OPERATIONS. THE COST OF FURNISHING, INSTALLING AND MAINTAINING THE POLYETHYLENE SHEETING SHALL BE INCIDENTAL TO THE UNIT PRICE BID FOR THE PTB.
- 21. EXCEPT FOR NECESSARY EQUIPMENT UTILIZED DURING PAVING OPERATIONS, NO CONSTRUCTION TRAFFIC OF ANY KIND SHALL BE PERMITTED TO RUN ON THE SOIL CEMENT BASE COURSE.

SECTION 600

- 22. THE DEPARTMENT AND THE CONTRACTOR SHALL REVIEW VIDEO INSPECTION OF ALL EXISTING PIPES AND DRAINAGE STRUCTURES TO BE USED IN THE FINAL DRAINAGE SYSTEM AND AGREE ON THE CONDITION PRIOR TO THE START OF CONSTRUCTION. EXISTING PIPES AND DRAINAGE STRUCTURES DAMAGED DUE TO CONTRACTOR OPERATIONS SHALL BE REPAIRED OR REPLACED IN-KIND AT THE CONTRACTOR'S EXPENSE. THE DEPARTMENT OR ITS REPRESENTATIVE WILL INSPECT NEW PIPE RUNS TO CONFIRM CONDITION PRIOR TO ACCEPTANCE.
- 23. ITEM 602002-P.C.C. MASONRY, CLASS B SHALL BE USED TO CONSTRUCT MISCELLANEOUS TYPES OF STRUCTURES SUCH AS PADS, BOLLARDS, ENCASEMENTS, ETC. AS DIRECTED BY THE ENGINEER UNLESS THE WORK IS TO BE PAID OTHERWISE AS INDICATED IN THE CONTRACT DOCUMENTS. THESE MISCELLANEOUS TYPES OF STRUCTURES ARE ANTICIPATED TO INVOLVE LESS THAN FIVE CUBIC YARDS PER SITE. THE VOLUME MEASURED FOR PAYMENT SHALL BE THE VOLUME OF P.C.C. MASONRY ACTUALLY PLACED TO CONSTRUCT THE MISCELLANEOUS STRUCTURE WITHIN THE LIMITS APPROVED BY THE ENGINEER. ALL COSTS ASSOCIATED WITH FURNISHING ALL LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK INCLUDING CONCRETE, REINFORCING STEEL, EXCAVATION, BACKFILL, BACKFILLING, ETC. SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 602002 - P.C.C. MASONRY CLASS B.
- 24. DRAINAGE INLETS TO BE ATTACHED TO EXISTING PIPES SHALL BE CAST IN PLACE. CAST IN PLACE CONSTRUCTION SHALL BE INCIDENTAL TO THE APPLICABLE BID ITEM FOR THE SUBJECT INLET.

SECTION 700

- 25. IN AREAS WHERE PROPOSED CURB MEETS EXISTING CURB AND THE TWO CURB TYPES ARE NOT SIMILAR, THE PROPOSED CURB SHALL BE TRANSDITIONED IN 10 LINEAR FEET, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. PAYMENT FOR THIS WORK, INCLUDING SAW CUTTING EXISTING CURB SHALL BE INCIDENTAL TO THE PROPOSED CURB ITEM.
- 26. PORTLAND CEMENT CONCRETE CHANNELIZING ISLANDS THAT ARE LESS THAN 75 SQ FT MAY BE POURED MONOLITHICALLY, OR AS DIRECTED BY THE ENGINEER.
- 27. STATION, OFFSET AND ELEVATION DATA GIVEN FOR DRAINAGE STRUCTURES ARE TO BE APPLIED TO THE CENTER OF THE GRATE ALONG THE FLOWLINE FOR INLETS, AND TO THE CENTER OF THE STRUCTURE FOR JUNCTION BOXES AND MANHOLES.
- 28. WHERE SPECIFIED ON THE PLANS, DRAINAGE INLET GRATES ADJACENT TO THE ROAD WHICH ARE NOT TYPE 1 SHALL BE REPLACED. THE ACTUAL LOCATIONS, THE NEED FOR ANY GRATE MODIFICATIONS OR FOR NEW FRAMES SHALL BE DETERMINED BY THE ENGINEER. ALL REPLACED GRATES/FRAMES SHALL BE DELIVERED TO THE NEAREST DISTRICT MAINTENANCE YARD WITH THE COST OF DELIVERY INCIDENTAL TO ITEM 708500 - REPLACING CATCH BASIN GRATES. FINAL PAYMENT FOR REPLACED GRATES/FRAMES SHALL NOT BE MADE UNTIL RECEIPT OF DELIVERED MATERIALS IS PRODUCED, SIGNED BY A DELDOT MAINTENANCE YARD SUPERVISOR.
- 29. ALL PAVED AREAS TO BE RECONSTRUCTED OR WIDENED SHALL BE SAWCUT AT THE POINT WHERE THE NEW PAVEMENT IS TO TIE INTO THE EXISTING PAVEMENT.
- 30. RAISED/RECESSED PAVEMENT MARKERS (RPM) SHALL BE INSTALLED IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL TITLED "DELAWARE DEPARTMENT OF TRANSPORTATION MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS" (PART 3) AND THE LATEST RPM GUIDELINES. PAYMENT FOR RPM INSTALLATION SHALL BE MADE UNDER ITEM 748502 - RAISED/RECESSED PAVEMENT MARKER.
- 31. ALL UNDERDRAIN OUTLETS, CATCH BASINS, PIPES, CONDUITS, JUNCTION WELLS, ETC. IN GUARDRAIL AREAS OR NEAR OTHER CONSTRUCTION YET TO BE PERFORMED SHALL BE VISIBLY MARKED BY THE CONTRACTOR AT THE TIME OF INSTALLATION IN ORDER TO AVOID FUTURE DAMAGE DURING DRIVING OF THE GUARDRAIL POSTS OR PERFORMANCE OF OTHER CONSTRUCTION. THE LOCATION OF GUARDRAIL POSTS AND OTHER CONSTRUCTION SHALL BE STAKED IN THE FIELD PRIOR TO PLACING THESE ITEMS. THE LOCATION OF THESE ITEMS SHALL BE ADJUSTED TO AVOID CONFLICTS WITH THE GUARDRAIL OR OTHER CONSTRUCTION. ALTERATIONS TO THE GUARDRAIL POST SPACING WILL NOT BE ALLOWED. ANY WORK REQUIRED TO RELOCATE THESE ITEMS DUE TO CONFLICTS WITH GUARDRAIL OR OTHER CONSTRUCTION SHALL BE PERFORMED TO THE SATISFACTION OF THE ENGINEER AND SHALL BE AT THE CONTRACTOR'S EXPENSE, INCLUDING ANY REMOVAL AND REPLACEMENT OF PAVEMENT.
- 32. DELDOT OR ITS REPRESENTATIVE SHALL FURNISH AND INSTALL RIGHT-OF-WAY MONUMENTS AFTER THE COMPLETION OF THE PROJECT. LOCATIONS OF RIGHT-OF-WAY MONUMENTS ARE PROVIDED ON THE PLANS FOR INFORMATION ONLY.
- 33. THE LOCATION FOR ITEM 759506 - FIELD OFFICE, TYPE II.22 SPECIAL COMPLEX SHALL BE ON THE DELDOT OWNED PARCEL EAST OF US 13 AT APPROXIMATE SR 1 STATION 1832+00. SEE DRAWING GR-02.
- 34. THE COST OF ANY FLOODLIGHTING NECESSARY DUE TO WORK BY THE CONTRACTOR ON ANY ITEM OCCURRING AFTER DARK SHALL BE INCIDENTAL TO THE BID PRICE OF THE ITEM BEING CONSTRUCTED AFTER DARK. DURING ALL CONSTRUCTION, ALL PERSONS WITHIN THE WORK ZONE SHALL HAVE SAFETY WEAR IN ACCORDANCE WITH THE DEMUTCD.
- 35. NO LESPEDEZA, ERAGROSTIS CURVULA, OR CORONILLA VARIA SHALL BE SEEDED. SECTION 734 - SEEDING HAS BEEN MODIFIED TO REMOVE LESPEDEZA, ERAGROSTIS CURVULA, AND CORONILLA VARIA.
- 36. INSTALLATION OF RIPRAP OUTLET PROTECTION (ITEMS 712005 AND 712006) SHALL BE IN ACCORDANCE WITH DIMENSIONS AND QUANTITIES INDICATED ON THE CONSTRUCTION PLANS. THE SPECIFIED DIMENSIONS ARE MINIMUM DIMENSIONS NECESSARY TO PROVIDE SUFFICIENT EROSION CONTROL. THE QUANTITY LISTED REPRESENTS THE SQUARE YARDAGE BASED UPON THE PLAN DEPICTION OF THE RIPRAP. DUE TO THE IRREGULAR CONFIGURATION OF SOME RIPRAP PADS, THE NOTED QUANTITY MAY NOT BE ACHIEVED BY A NOMINAL AMOUNT NOT TO EXCEED 5% LESS THAN THE NOTED QUANTITY. THE ENGINEER SHALL APPROVE ALL RIPRAP INSTALLATION. THE COST FOR SUPPLY AND INSTALLATION OF ALL STONE BEDDING BELOW PROPOSED RIPRAP SHALL BE INCIDENTAL TO THE ASSOCIATED RIPRAP ITEM.

PROJECT NOTES (CONT.)

SECTION 700 (CONT.)

- 37. ITEM 727000 - RIGHT-OF-WAY FENCE SHALL BE INSTALLED BY HAND IN SENSITIVE AREAS. SENSITIVE AREAS INCLUDE WOODS, WETLANDS, STREAMS, CULTURAL RESOURCE AREAS AND OTHER AREAS AS SHOWN ON THE PLANS AND AS DETERMINED BY THE ENGINEER. THERE SHALL BE NO VEHICLE ACCESS AND GRUBBING FOR THE PURPOSES OF INSTALLING RIGHT-OF-WAY FENCE IN SENSITIVE AREAS. CLEARING OF VEGETATION FOR THE PURPOSE OF INSTALLING RIGHT-OF-WAY FENCE SHALL BE KEPT TO A MINIMUM IN SENSITIVE AREAS. IF REMOVAL OF VEGETATION CANNOT BE AVOIDED, THE VEGETATION SHALL BE CUT FLUSH WITH THE GROUND SURFACE (I.E., NO DISTURBANCE OF THE ROOT MAT). HAND-MIXED CONCRETE SHALL BE USED FOR CONCRETE FOOTINGS IN SENSITIVE AREAS. POST SPACING SHALL BE ADJUSTED AS APPROVED BY THE ENGINEER TO COMPLY WITH THE MINIMUM AND MAXIMUM CLEARANCE OF THE BOTTOM OF THE FABRIC. NO EXCAVATION OR BACKFILLING OF THE EXISTING GROUND SHALL BE CONDUCTED TO COMPLY WITH THE MINIMUM AND MAXIMUM CLEARANCE OF THE BOTTOM OF FABRIC OVER GROUND IN SENSITIVE AREAS. EXCAVATIONS FOR POSTS AND FOOTERS WITHIN SENSITIVE AREAS THAT WILL BE USED FOR BACKFILLING OF THE POSTS AND FOOTERS SHALL BE PLACED ON PLASTIC AND ANY EXCESS EXCAVATIONS SHALL BE REMOVED AND DISPOSED OF IN NON-SENSITIVE AREAS AS APPROVED BY THE ENGINEER.

SECTION 900

- 38. THIS PROJECT IS COVERED UNDER AN NPDES GENERAL PERMIT FOR CONSTRUCTION. UNDER THE GENERAL PERMIT, COMPLIANCE WITH DELDOT'S APPROVED SEDIMENT AND STORMWATER MANAGEMENT PLANS WILL CONSTITUTE COMPLIANCE WITH THE NPDES INDUSTRIAL PERMITTING REQUIREMENTS FOR THIS CONSTRUCTION PROJECT. A COPY OF THE NPDES GENERAL PERMIT AND NOIS IS KEPT ON FILE IN EACH OF THE CONSTRUCTION OFFICES AND THE DEPARTMENT'S STORMWATER SECTION. A COPY OF THE GENERAL PERMIT OR THE NOIS CAN BE OBTAINED UPON REQUEST FROM EITHER THE DEPARTMENT'S STORMWATER ENGINEER OR THE APPROPRIATE CONSTRUCTION ENGINEER.

MISCELLANEOUS

- 39. THE CONTRACTOR SHALL CONTACT MICHAEL ELLER, THE CHIEF OF SCHEDULING FOR DART FIRST STATE, 14 DAYS PRIOR TO THE START OF CONSTRUCTION AT 302-576-6061.
- 40. ANY CHANGES TO OR DEVIATIONS FROM THESE PLANS REQUESTED BY THE CONTRACTOR MUST BE REVIEWED AND APPROVED BY THE ENGINEER AND ENVIRONMENTAL MONITOR PRIOR TO CONDUCTING ANY WORK. APPROVAL MAY TAKE A SIGNIFICANT AMOUNT OF TIME TO COMPLETE AND ALL CHANGES MAY NOT BE APPROVED. THE CONTRACTOR SHALL HAVE NO CLAIM AGAINST THE DEPARTMENT FOR COSTS OR DELAYS ASSOCIATED WITH THE APPROVAL OR REJECTION OF REQUESTED CHANGES OR DEVIATIONS FROM THESE PLANS.
- 41. RESTORATION OF TEMPORARY IMPACTS
  - A. PRIOR TO PERFORMING ANY WORK ASSOCIATED WITH TEMPORARY IMPACTS TO DELINEATED WETLANDS, THE CONTRACTOR SHALL STAKE THE LIMITS OF TEMPORARY DISTURBANCE WITHIN THE WETLANDS AND ALLOW 14 CALENDAR DAYS FOR DELDOT TO OBTAIN EXISTING TOPOGRAPHY SURVEY WITHIN THE TEMPORARY DISTURBANCE. THIS EXISTING SURFACE SHALL BE PROVIDED TO AND ACCEPTED BY THE CONTRACTOR BEFORE ANY WORK IS PERFORMED WITHIN THE WETLANDS. THE CONTRACTOR SHALL HAVE 5 CALENDAR DAYS TO RESPOND TO THE EXISTING SURFACE INFORMATION OR OTHERWISE IT SHALL BE CONSIDERED ACCEPTED. THE EXISTING SURFACE PLAN SHALL BE PROVIDED IN BOTH DIGITAL AND PAPER COPIES CONFORMING TO DELDOT CADD STANDARDS AT THE SAME SCALE AS THE CONTRACT PLANS.
  - B. UPON MUTUAL ACCEPTANCE OF THE EXISTING SURFACE TOPOGRAPHY PLAN, THE CONTRACTOR SHALL FIRST INSTALL THE RESOURCE PROTECTION FENCE, AND THEN INSTALL THE NECESSARY EROSION AND SEDIMENT CONTROL DEVICES AS SHOWN ON THE PLANS AND DIRECTED BY THE ENGINEER. THE AREA OF THE TEMPORARY DISTURBANCE MAY BE CLEARED OF VEGETATION AS NECESSARY. VEGETATION SHALL NOT BE GRUBBED, AND SHALL BE CUT FLUSH WITH THE GROUND (I.E., NO DISTURBANCE OF THE ROOT MAT).
  - C. INSTALL THE TEMPORARY ACCESS ROAD OR OTHER NEEDED TEMPORARY DISTURBANCE AS SHOWN ON THE PLANS OR APPROVED BY THE ENGINEER. GEOTEXTILE SHALL BE PLACED ON TOP OF THE EXISTING GROUND TO PROVIDE SEPARATION BETWEEN THE EXISTING GROUND AND ANY PLACED MATERIALS.
  - D. WHEN THE CONTRACTOR HAS COMPLETED THE WORK REQUIRING THE TEMPORARY WETLAND DISTURBANCE, ALL MATERIALS THAT WERE PLACED BY THE CONTRACTOR SHALL BE REMOVED IN THEIR ENTIRETY. ONCE ALL MATERIALS HAVE BEEN REMOVED, THE CONTRACTOR SHALL ALLOW 14 CALENDAR DAYS FOR DELDOT TO OBTAIN EXISTING SURFACE ELEVATIONS OF THE DISTURBED AREA FOLLOWING THE SAME PROCEDURE DESCRIBED ABOVE FOR OBTAINING ORIGINAL ELEVATIONS. THESE EXISTING SURFACE ELEVATIONS SHALL BE PROVIDED TO THE CONTRACTOR AND INCLUDE A PLAN SHOWING THE ELEVATION DIFFERENCES BETWEEN THE ORIGINAL AND EXISTING SURFACES.
  - E. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORING THE TEMPORARY DISTURBED AREA TO ORIGINAL ELEVATIONS WITH A GRADING TOLERANCE OF PLUS OR MINUS 0.1 FEET. RESTORATION OF THE DISTURBED AREA SHALL BE ACCOMPLISHED IN THE FOLLOWING MANNER:
    - I. TILL THE GROUND WITHIN THE DISTURBED AREA TO LOOSEN UP THE SOILS DUE TO COMPACTION DURING CONSTRUCTION IN ACCORDANCE WITH THE SPECIFICATIONS OF ITEM 202555 - SUBSOIL TILLAGE. MINIMUM VERTICAL TILLAGE DEPTH SHALL BE 24 INCHES AS MEASURED BY FIELD PERFORMANCE.
    - II. PLACE TOPSOIL TO FILL DEPRESSIONS TO THE ORIGINAL GROUND ELEVATIONS. TOPSOIL SHALL BE THE TOP 9 INCHES OF SOIL OBTAINED FROM AN ACTIVE OR RECENTLY (LESS THAN TWO YEARS) FALLOW OR ABANDONED CROP PRODUCING FARM FIELD OR A SANDY LOAM WITH A MINIMUM OF 4% ORGANIC MATTER. MAXIMUM DEPTH OF A SINGLE LIFT OF TOPSOIL PLACED SHALL BE 6 INCHES AND SHALL BE PLACED IN ACCORDANCE WITH SECTION 732.
    - III. DISK THE FINAL TOPSOIL SURFACE WITHIN THE DISTURBED AREA TO PREPARE THE AREA FOR SEED. USE A MINIMUM OF 3 PASSES OF A DISK USING LOW GROUND PRESSURE EQUIPMENT TO A MINIMUM DEPTH OF 4 INCHES.
    - IV. WHEN THE CONTRACTOR BELIEVES THAT RESTORATION OF THE ORIGINAL ELEVATIONS HAS BEEN ACHIEVED, 7 CALENDAR DAYS SHALL BE ALLOWED FOR THE AREA TO AGAIN BE SURVEYED BY DELDOT UNDER THE SAME CONDITIONS DESCRIBED ABOVE AND THE SURVEY PLAN OF THE RESTORED ELEVATIONS WILL BE PROVIDED TO THE CONTRACTOR. DELDOT SHALL ADVISE THE CONTRACTOR IF ADDITIONAL RESTORATION WORK IS REQUIRED AND THE CONTRACTOR SHALL ADDRESS THOSE AREAS AND ALLOW FOR 7 CALENDAR DAYS FOR NEW SURVEY INFORMATION TO BE OBTAINED UNTIL THE RESTORATION IS APPROVED BY DELDOT.
  - F. UPON ACCEPTANCE OF THE RESTORED ELEVATIONS, THE CONTRACTOR SHALL APPLY SEED TO THE DISTURBED WETLAND. SEEDING SHALL VARY BASED ON THE SLOPE TO BE SEEDED, ON SLOPES 5:1 OR FLATTER, SEEDING SHALL BE CONDUCTED UNDER ITEM 734552 - WET GROUND EROSION CONTROL GRASS SEEDING - FLATS. ON SLOPES GREATER THAN 5:1, SEEDING SHALL BE CONDUCTED UNDER ITEM 734013 - PERMANENT GRASS SEEDING, DRY GROUND.
  - G. THE RESTORED AREAS WITHIN THE LIMITS OF THE DELINEATED WETLANDS SHALL BE PLANTED IN ACCORDANCE WITH ITEM 737523 - PLANTING. SMOOTH ALDER SHALL BE PLANTED 10 FOOT ON CENTER ON SLOPES FLATTER THAN 5:1 AND SOUTHERN ARROWWOOD SHALL BE PLANTED 10 FOOT ON CENTER ON SLOPES STEEPER THAN 5:1. PLANTS SHALL BE INSTALLED DURING THE FIRST AVAILABLE PLANTING WINDOW PER THE STANDARD SPECIFICATIONS. SHRUBS SHALL NOT BE PLANTED UNDER BRIDGES. BEGIN SHRUB PLANTING 10 FEET OUTSIDE OF THE BRIDGE PARAPETS.
  - H. UPON FINAL ACCEPTANCE OF THE PLANTING, THE CONTRACTOR SHALL REMOVE THE RESOURCE PROTECTION FENCING AND THE EROSION AND SEDIMENT CONTROL MEASURES.
  - I. ALL COSTS FOR INSTALLING, REMOVING, AND RESTORING THE TEMPORARY WETLAND ACCESS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 202508 - WETLAND ACCESS ROAD, TYPE II, WITH THE EXCEPTION OF THE RESOURCE PROTECTION FENCE, WHICH SHALL BE PAID UNDER ITEM 727552, AND THE REINFORCED SILT FENCE, WHICH SHALL BE PAID UNDER ITEM 251001. MAINTENANCE OF STREAM FLOW ASSOCIATED WITH THE WETLAND ACCESS ROAD, BRIDGE CONSTRUCTION AND ASSOCIATED ACTIVITIES TO BE PERFORMED IN WHOLE OR IN PART FROM THE WETLAND ACCESS ROAD, AND STREAM AND WETLAND RESTORATION ACTIVITIES SHALL BE INCIDENTAL TO THE WETLAND ACCESS ROAD, TYPE II.

PROJECT NOTES (CONT.)

MISCELLANEOUS (CONT.)

- 42. RESTORATION OF PERMANENT IMPACTS
  - A. PERMANENT IMPACTS TO CLEARED AND GRUBBED WETLANDS THAT HAVE NOT BEEN GRADED SHALL BE RESTORED WITH SEEDING AND SHRUB PLANTING AS INDICATED ON THE PLANS. SEEDING AND PLANTING SHALL BE CONDUCTED BETWEEN THE LIMITS OF GRADING AND THE LOC IN LOCATIONS DESIGNATED ON THE PLANS.
  - B. SEEDING SHALL VARY BASED ON SLOPE TO BE SEEDED. ON SLOPES 5:1 OR FLATTER, SEEDING SHALL BE PAID FOR AND CONDUCTED UNDER ITEM 734552 - WET GROUND EROSION CONTROL GRASS SEEDING - FLATS. ON SLOPES GREATER THAN 5:1 SEEDING SHALL PAID FOR AND CONDUCTED UNDER ITEM 734013 - PERMANENT GRASS SEEDING, DRY GROUND.
  - C. SHRUBS SHALL BE PLANTED IN THE PERMANENT IMPACT RESTORATION AREA. THE SHRUB PLANTING WILL VARY BASED ON SLOPE OF THE PLANTED AREA. ON SLOPES 5:1 OR FLATTER, SHRUB PLANTING SHALL CONSIST OF CONTAINERIZE 3 TO 5 FOOT TALL SMOOTH ALDER (ALNUS SERRULATA) LOCATED 10 FOOT ON CENTER. ON SLOPES GREATER THAN 5:1 SHRUB PLANTING SHALL CONSIST OF CONTAINERIZED 3 TO 5 FOOT TALL SOUTHERN ARROWWOOD (VIBURNUM DENTATUM) LOCATED 10 FOOT ON CENTER. PERMANENT IMPACT RESTORATION SHRUB PLANTING SHALL BE PAID FOR AND CONDUCTED UNDER ITEM 737523 - PLANTING.
- 43. IF GROWTH OF A NOXIOUS WEED AS DEFINED IN TITLE 3 OF THE DELAWARE CODE OR AS IDENTIFIED BY THE ENGINEER IS DETECTED WITHIN THE PROJECT LIMITS AS A RESULT OF USING ON-SITE MATERIALS, THE CONTRACTOR SHALL ERADICATE THE WEED USING ITEM 735501 HERBICIDE APPLICATION, NOXIOUS WEEDS. GROWTH OF NOXIOUS WEEDS RESULTING FROM THE CONTRACTOR BRINGING MATERIALS TO THE PROJECT FROM OFF-SITE SOURCES SHALL BE ERADICATED AT THE CONTRACTOR'S EXPENSE.
- 44. THE CONTRACTOR SHALL FOLLOW ALL STATE AND LOCAL ORDINANCES CONCERNING CONSTRUCTION NOISE DURING THE DURATION OF THE CONSTRUCTION ACTIVITIES.
- 45. EXCAVATION WITHIN WOODED AREAS SHALL BE INCIDENTAL TO ITEM 201000 - CLEARING AND GRUBBING FOR A DEPTH OF 2 FEET BELOW THE EXISTING GRADE WITHIN THE ROADWAY FOOTPRINT OR STORMWATER MANAGEMENT FACILITY FOOTPRINT. ALL OTHER EXCAVATION SHALL BE PAID FOR UNDER ITEM 202000 - EXCAVATION AND EMBANKMENT.

RIGHT-OF-WAY MONUMENT SCHEDULE				
NO.	STATION	OFFSET	NORTHING	EASTING
1	1015+69.14	145.71	557558.9940	590088.9694
2	1015+20.00	385.00	557680.2870	589878.1085
3	1011+50.00	385.00	557981.9571	589956.9405
4	1009+81.26	175.00	558099.3107	590186.3460
5	1007+00.00	175.00	558374.4565	590213.4396
6	1006+00.00	87.37	558465.8978	590308.6433
7	851+50.00	176.91	556403.7354	589971.5255
8	851+50.00	148.00	556426.3170	589953.4759
9	151+50.00	-72.00	557380.1943	590570.3602
10	156+50.00	-47.00	557930.0071	590599.1854
11	159+26.77	-47.00	558206.7668	590601.1106
12	162+76.00	-47.00	558556.6712	590606.2107
13	172+00.00	-47.00	559480.4485	590626.7158
14	175+25.00	-70.78	559805.8924	590610.1513

DESIGN DESIGNATION - RAMP R		
FUNCTIONAL CLASS: N/A	D.H.V. PROJECTED: 1,200	YEAR: 2030
TYPE OF CONSTRUCTION: NEW CONSTRUCTION		DESIGN SPEED: 50 M.P.H.
A.A.D.T. CURRENT: N/A	YEAR: N/A	TRUCKS: 6%
A.A.D.T. PROJECTED: 14,000	YEAR: 2030	DIRECTION OF DISTRIBUTION: N/A
DESIGN DESIGNATION - US 13 (N22)		
FUNCTIONAL CLASS: MINOR ARTERIAL	D.H.V. PROJECTED: 4,275	YEAR: 2030
TYPE OF CONSTRUCTION: REALIGNMENT		DESIGN SPEED: 65 M.P.H.
A.A.D.T. CURRENT: 24,318	YEAR: 2008	TRUCKS: 7%
A.A.D.T. PROJECTED: 43,600	YEAR: 2030	DIRECTION OF DISTRIBUTION: 50%
DESIGN DESIGNATION - SR 1 (N83)		
FUNCTIONAL CLASS: OTHER PRINCIPAL ARTERIAL	D.H.V. PROJECTED: 8,950	YEAR: 2030
TYPE OF CONSTRUCTION: WIDENING		DESIGN SPEED: 70 M.P.H.
A.A.D.T. CURRENT: 71,024	YEAR: 2008	TRUCKS: 13%
A.A.D.T. PROJECTED: 110,000	YEAR: 2030	DIRECTION OF DISTRIBUTION: 50%
<b>FOR DESIGN DESIGNATION - RAMP Q AND DESIGN DESIGNATION - US 301 NB RAMP, SEE DESIGN DESIGNATION - US 301 ON THE TITLE SHEET</b>		

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ADDENDUMS / REVISIONS	
△	REVISED NOTE: BRT/DMG

**NOT TO SCALE**

**US 301 &  
SR 1 INTERCHANGE**

CONTRACT	BRIDGE NO.
T200911302	
COUNTY	DESIGNED BY: SJB
NEW CASTLE	CHECKED BY: TAO

NOTES	

<b>PN-02</b>
SHEET NO.
7
TOTAL SHTS.
491

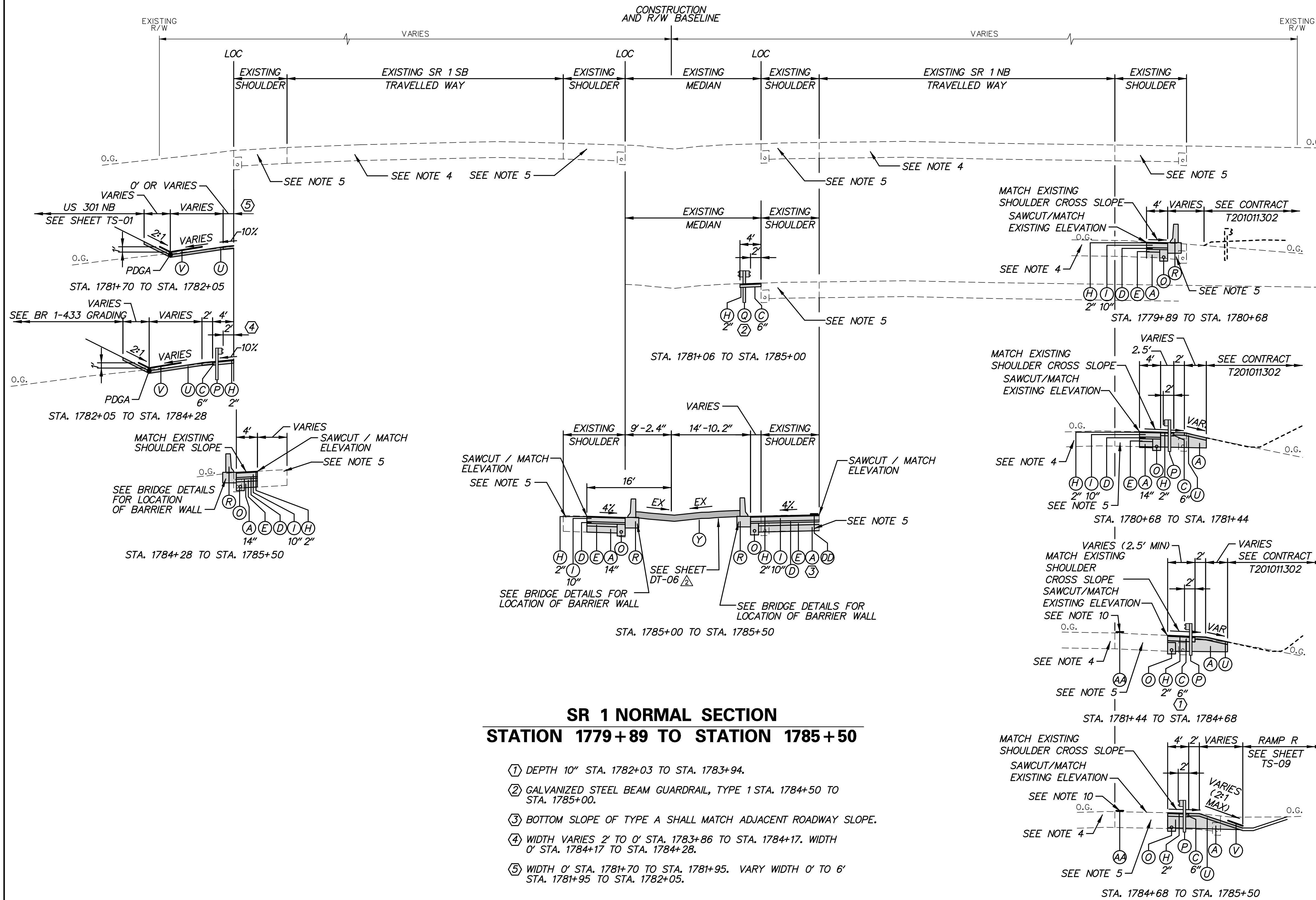


**LEGEND**

- (A) ITEM 209001 - BORROW, TYPE A
- (B) ITEM 209006 - BORROW, TYPE F
- (C) ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B
- (D) ITEM 304501 - PERMEABLE TREATED BASE, 4"
- (E) ITEM 304502 - SOIL CEMENT BASE COURSE, 6"
- (G) ITEM 401813 - WMA, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 70-22 (2 LIFTS)
- (H) ITEM 401801 - WMA, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22 (CARBONATE STONE)
- (I) ITEM 401819 - WMA, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22
- (J) ITEM 401824 - WMA, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22 WEDGE
- (K) ITEM 501006 - PORTLAND CEMENT CONCRETE PAVEMENT, 12"
- (L) ITEM 732002 - TOPSOILING, 6" DEPTH  
ITEM 734551 - NATIVE GRASS SEEDING: NO MOW MIX
- (N) ITEM 701016 - PCC CURB AND GUTTER TYPE 1-4
- (O) ITEM 715001 - PERFORATED PIPE UNDERDRAINS, 6"
- (P) ITEM 720050 - GALVANIZED STEEL BEAM GUARDRAIL, TYPE 1-31
- (Q) ITEM 720052 - GALVANIZED STEEL BEAM GUARDRAIL, TYPE 3-31
- (R) ITEM 720626 - CONCRETE SINGLE FACE BARRIER, TYPE 1  
ITEM 720627 - CONCRETE SINGLE FACE BARRIER, TYPE 2 (MODIFIED)
- (S) ITEM 727000 - RIGHT OF WAY FENCE
- (T) ITEM 705002 - P.C.C. SIDEWALK, 6"
- (U) ITEM 732002 - TOPSOILING, 6" DEPTH  
ITEM 734013 - PERMANENT GRASS SEEDING, DRY GROUND
- (V) ITEM 732002 - TOPSOILING, 6" DEPTH  
ITEM 734013 - PERMANENT GRASS SEEDING, DRY GROUND  
ITEM 735535 - SOIL RETENTION BLANKET MULCH, TYPE 5
- (W) ITEM 760003 - PAVEMENT MILLING, HOT-MIX, VARIABLE DEPTH
- (X) ITEM 760507 - PROFILE MILLING, HOT-MIX
- (Y) ITEM 712005 - RIPRAP, R-4
- (Z) ITEM 720629 - BIFURCATED CONCRETE MEDIAN BARRIER (MODIFIED)
- (AA) ITEM 760017 - RUMBLE STRIPS, CONCRETE
- (BB) ITEM 760012 - RUMBLE STRIPS, BIKE FRIENDLY, HOT-MIX
- (CC) ITEM 760015 - RUMBLE STRIPS, CONCRETE SHALLOW DEPTH
- (DD) ITEM 760016 - RUMBLE STRIPS, HOT-MIX

**TYPICAL SECTION NOTES:**

1. THE MAXIMUM ALGEBRAIC DIFFERENCE ON THE HIGH SIDE BETWEEN THE TRAVELLED WAY SLOPE AND THE ACCEL/DECEL LANES SHALL NOT EXCEED 4%. THE MAXIMUM ALGEBRAIC DIFFERENCE BETWEEN THE TRAVELLED WAY SLOPE AND THE SHOULDER SLOPE SHALL NOT EXCEED 8%.
2. SHOULDER SLOPE ON THE LOW SIDE SHALL BE THE SAME AS THE TRAVELLED WAY SLOPE WHEN SUPERELEVATION IS GREATER THAN 4%.
3. P.G.L. - PROFILE GRADE LINE  
PGA - POINT OF GRADE APPLICATION  
PDGA - POINT OF DITCH GRADE APPLICATION  
P/R - POINT OF ROTATION  
O.G. - ORIGINAL GROUND
4. EXISTING SR 1 ROADWAY PAVEMENT SECTION  
12" PCC PAVEMENT  
4" PERMEABLE ASPHALT TREATED BASE  
6" SOIL CEMENT TREATED BASE  
14" BORROW TYPE A
5. EXISTING SR 1 SHOULDER ADJACENT SECTION  
2" HOT MIX, HOT LAID BITUMINOUS CONCRETE PAVEMENT, TYPE C  
10"+/- BITUMINOUS CONCRETE BASE COURSE  
4" PERMEABLE ASPHALT TREATED BASE  
6" SOIL CEMENT TREATED BASE  
14" BORROW TYPE A
6. FOR BENCHING OF EXISTING SLOPES WHERE PROPOSED WIDENING OCCURS, SEE BENCHING DETAIL, SHEET DT-01.
7. SEE GRADES AND GEOMETRICS SHEETS FOR SUPERELEVATED CROSS SLOPE TRANSITIONS.
8. SEE SHEET TS-20 FOR UNDERDRAIN DETAIL. SEE CONSTRUCTION PLANS FOR PLACEMENT LOCATIONS
9. THE MAXIMUM LIFTS FOR THE INDIVIDUAL PAVING MATERIALS ARE AS FOLLOWS:  
SUPERPAVE, TYPE C WMA - 2"  
SUPERPAVE, TYPE B WMA - 3", PLACE IN EQUAL LIFTS WHEN THICKNESS EXCEEDS 3"  
SUPERPAVE BIT. CONC. BASE COURSE - 4"  
GRADED AGGREGATE BASE COURSE - 8"
10. SEE SHEET DT-01 FOR RUMBLE STRIP LOCATIONS.
11. SEE SHEET DT-01 FOR SAFETY EDGE DETAILS. THESE DETAILS SHALL BE APPLIED AT ALL INSTANCES WHERE THE EDGE OF PROPOSED PAVEMENT MEETS THE 6" TOPSOIL.



**SR 1 NORMAL SECTION  
STATION 1779+89 TO STATION 1785+50**

- (1) DEPTH 10" STA. 1782+03 TO STA. 1783+94.
- (2) GALVANIZED STEEL BEAM GUARDRAIL, TYPE 1 STA. 1784+50 TO STA. 1785+00.
- (3) BOTTOM SLOPE OF TYPE A SHALL MATCH ADJACENT ROADWAY SLOPE.
- (4) WIDTH VARIES 2' TO 0' STA. 1783+86 TO STA. 1784+17. WIDTH 0' STA. 1784+17 TO STA. 1784+28.
- (5) WIDTH 0' STA. 1781+70 TO STA. 1781+95. VARY WIDTH 0' TO 6' STA. 1781+95 TO STA. 1782+05.

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ADDENDUMS / REVISIONS	
△ REVISED CROSS-REFERENCE: BRT/DMG	

**NOT TO SCALE**

**US 301 &  
SR 1 INTERCHANGE**

CONTRACT T200911302	BRIDGE NO.
COUNTY NEW CASTLE	DESIGNED BY: S.E.S.
	CHECKED BY: B.R.T.

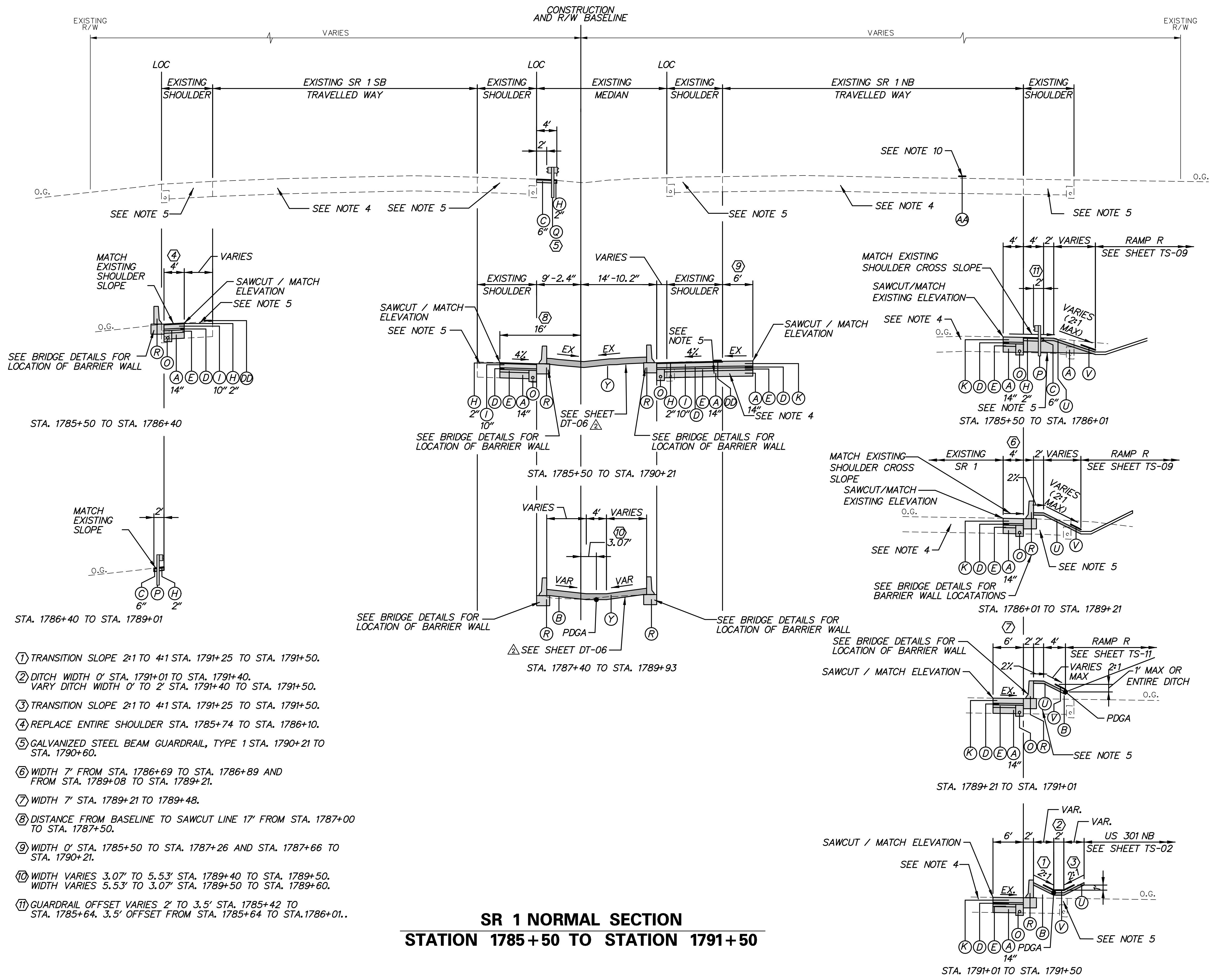
**TYPICAL SECTIONS**

SHEET NO.	23
TOTAL SHTS.	491

**LEGEND**

- (A) ITEM 209001 - BORROW, TYPE A
- (B) ITEM 209006 - BORROW, TYPE F
- (C) ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B
- (D) ITEM 304501 - PERMEABLE TREATED BASE, 4"
- (E) ITEM 304502 - SOIL CEMENT BASE COURSE, 6"
- (G) ITEM 401813 - WMA, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 70-22 (2 LIFTS)
- (H) ITEM 401801 - WMA, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22 (CARBONATE STONE)
- (I) ITEM 401819 - WMA, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22
- (J) ITEM 401824 - WMA, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22 WEDGE
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- (Q) ITEM 720052 - GALVANIZED STEEL BEAM GUARDRAIL, TYPE 3-31
- (R) ITEM 720626 - CONCRETE SINGLE FACE BARRIER, TYPE 1  
ITEM 720626 - CONCRETE SINGLE FACE BARRIER, TYPE 1 (MODIFIED)  
ITEM 720627 - CONCRETE SINGLE FACE BARRIER, TYPE 2
- (S) ITEM 727000 - RIGHT OF WAY FENCE
- (T) ITEM 705002 - P.C.C. SIDEWALK, 6"
- (U) ITEM 732002 - TOPSOILING, 6" DEPTH  
ITEM 734013 - PERMANENT GRASS SEEDING, DRY GROUND
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- (X) ITEM 760507 - PROFILE MILLING, HOT-MIX
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- (Z) ITEM 720629 - BIFURCATED CONCRETE MEDIAN BARRIER (MODIFIED)
- (AA) ITEM 760017 - RUMBLE STRIPS, CONCRETE
- (BB) ITEM 760012 - RUMBLE STRIPS, BIKE FRIENDLY, HOT-MIX
- (CC) ITEM 760015 - RUMBLE STRIPS, CONCRETE SHALLOW DEPTH
- (DD) ITEM 760016 - RUMBLE STRIPS, HOT-MIX

- TYPICAL SECTION NOTES:**
1. THE MAXIMUM ALGEBRAIC DIFFERENCE ON THE HIGH SIDE BETWEEN THE TRAVELED WAY SLOPE AND THE ACCEL/DECEL LANES SHALL NOT EXCEED 4%. THE MAXIMUM ALGEBRAIC DIFFERENCE BETWEEN THE TRAVELED WAY SLOPE AND THE SHOULDER SLOPE SHALL NOT EXCEED 8%.
  2. SHOULDER SLOPE ON THE LOW SIDE SHALL BE THE SAME AS THE TRAVELED WAY SLOPE WHEN SUPERELEVATION IS GREATER THAN 4%.
  3. P.G.L. - PROFILE GRADE LINE  
PGA - POINT OF GRADE APPLICATION  
PDGA - POINT OF DITCH GRADE APPLICATION  
P/R - POINT OF ROTATION  
O.G. - ORIGINAL GROUND
  4. EXISTING SR 1 ROADWAY PAVEMENT SECTION  
12" PCC PAVEMENT  
4" PERMEABLE ASPHALT TREATED BASE  
6" SOIL CEMENT TREATED BASE  
14" BORROW TYPE A
  5. EXISTING SR 1 SHOULDER ADJACENT SECTION  
2" HOT MIX, HOT LAID BITUMINOUS CONCRETE PAVEMENT, TYPE C  
10"+/- BITUMINOUS CONCRETE BASE COURSE  
4" PERMEABLE ASPHALT TREATED BASE  
6" SOIL CEMENT TREATED BASE  
14" BORROW TYPE A
  6. FOR BENCHING OF EXISTING SLOPES WHERE PROPOSED WIDENING OCCURS, SEE BENCHING DETAIL, SHEET DT-01.
  7. SEE GRADES AND GEOMETRICS SHEETS FOR SUPERELEVATED CROSS SLOPE TRANSITIONS.
  8. SEE SHEET TS-20 FOR UNDERDRAIN DETAIL.  
SEE CONSTRUCTION PLANS FOR PLACEMENT LOCATIONS
  9. THE MAXIMUM LIFTS FOR THE INDIVIDUAL PAVING MATERIALS ARE AS FOLLOWS:  
SUPERPAVE, TYPE C WMA - 2"  
SUPERPAVE, TYPE B WMA - 3", PLACE IN EQUAL LIFTS WHEN THICKNESS EXCEEDS 3"  
SUPERPAVE BIT. CONC. BASE COURSE - 4"  
GRADED AGGREGATE BASE COURSE - 8"
  10. SEE SHEET DT-01 FOR RUMBLE STRIP LOCATIONS.
  11. SEE SHEET DT-01 FOR SAFETY EDGE DETAILS. THESE DETAILS SHALL BE APPLIED AT ALL INSTANCES WHERE THE EDGE OF PROPOSED PAVEMENT MEETS THE 6" TOPSOIL.



**SR 1 NORMAL SECTION  
STATION 1785+50 TO STATION 1791+50**

- (1) TRANSITION SLOPE 2:1 TO 4:1 STA. 1791+25 TO STA. 1791+50.
- (2) DITCH WIDTH 0' STA. 1791+01 TO STA. 1791+40.  
VARY DITCH WIDTH 0' TO 2' STA. 1791+40 TO STA. 1791+50.
- (3) TRANSITION SLOPE 2:1 TO 4:1 STA. 1791+25 TO STA. 1791+50.
- (4) REPLACE ENTIRE SHOULDER STA. 1785+74 TO STA. 1786+10.
- (5) GALVANIZED STEEL BEAM GUARDRAIL, TYPE 1 STA. 1790+21 TO STA. 1790+60.
- (6) WIDTH 7' FROM STA. 1786+69 TO STA. 1786+89 AND FROM STA. 1789+08 TO STA. 1789+21.
- (7) WIDTH 7' STA. 1789+21 TO 1789+48.
- (8) DISTANCE FROM BASELINE TO SAWCUT LINE 17' FROM STA. 1787+00 TO STA. 1787+50.
- (9) WIDTH 0' STA. 1785+50 TO STA. 1787+26 AND STA. 1787+66 TO STA. 1790+21.
- (10) WIDTH VARIES 3.07' TO 5.53' STA. 1789+40 TO STA. 1789+50.  
WIDTH VARIES 5.53' TO 3.07' STA. 1789+50 TO STA. 1789+60.
- (11) GUARDRAIL OFFSET VARIES 2' TO 3.5' STA. 1785+42 TO STA. 1785+64. 3.5' OFFSET FROM STA. 1785+64 TO STA. 1786+01.

ADDENDUMS / REVISIONS


**NOT TO SCALE**

**US 301 &  
SR 1 INTERCHANGE**

CONTRACT T200911302	BRIDGE NO. 
COUNTY NEW CASTLE	DESIGNED BY: J.A.D.
	CHECKED BY: B.R.T.

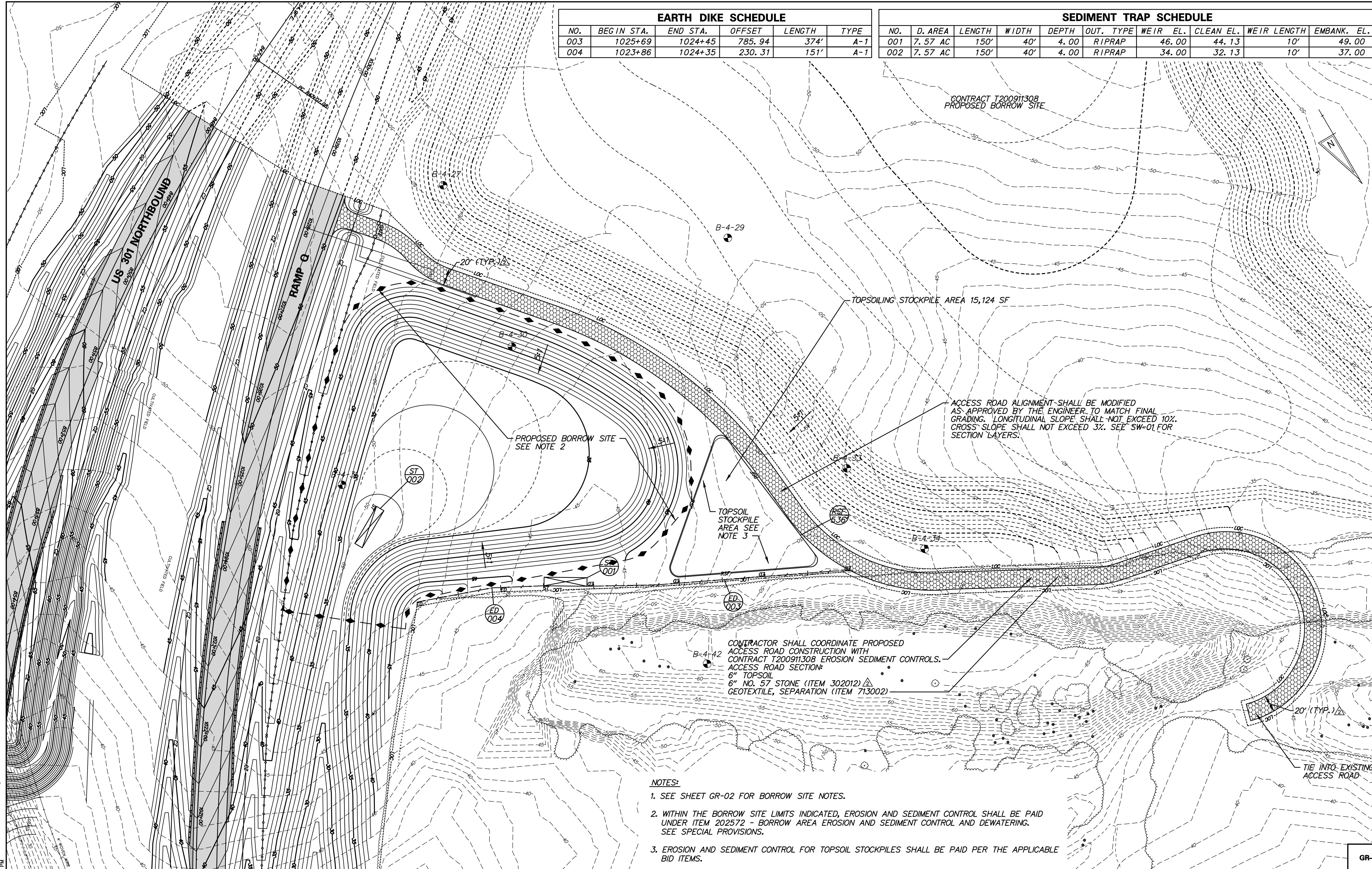
<b>TYPICAL SECTIONS</b>	SHEET NO. 24
	TOTAL SHTS. 491

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EARTH DIKE SCHEDULE					
NO.	BEGIN STA.	END STA.	OFFSET	LENGTH	TYPE
003	1025+69	1024+45	785.94	374'	A-1
004	1023+86	1024+35	230.31	151'	A-1

SEDIMENT TRAP SCHEDULE									
NO.	D. AREA	LENGTH	WIDTH	DEPTH	OUT. TYPE	WEIR EL.	CLEAN EL.	WEIR LENGTH	EMBANK. EL.
001	7.57 AC	150'	40'	4.00	RIPRAP	46.00	44.13	10'	49.00
002	7.57 AC	150'	40'	4.00	RIPRAP	34.00	32.13	10'	37.00



CONTRACT T200911308  
PROPOSED BORROW SITE

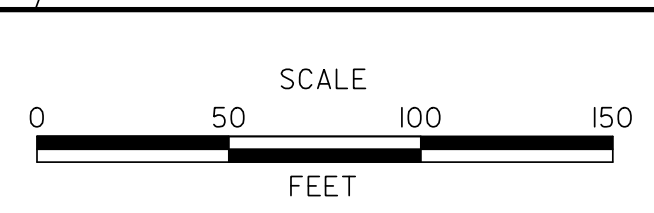
ACCESS ROAD ALIGNMENT SHALL BE MODIFIED AS APPROVED BY THE ENGINEER TO MATCH FINAL GRADING. LONGITUDINAL SLOPE SHALL NOT EXCEED 10%. CROSS SLOPE SHALL NOT EXCEED 3%. SEE SW-01 FOR SECTION LAYERS.

CONTRACTOR SHALL COORDINATE PROPOSED ACCESS ROAD CONSTRUCTION WITH CONTRACT T200911308 EROSION SEDIMENT CONTROLS. ACCESS ROAD SECTION:  
6" TOPSOIL  
6" NO. 57 STONE (ITEM 302012)  $\Delta$   
GEOTEXTILE, SEPARATION (ITEM 713002)

- NOTES:**
- SEE SHEET GR-02 FOR BORROW SITE NOTES.
  - WITHIN THE BORROW SITE LIMITS INDICATED, EROSION AND SEDIMENT CONTROL SHALL BE PAID UNDER ITEM 202572 - BORROW AREA EROSION AND SEDIMENT CONTROL AND DEWATERING. SEE SPECIAL PROVISIONS.
  - EROSION AND SEDIMENT CONTROL FOR TOPSOIL STOCKPILES SHALL BE PAID PER THE APPLICABLE BID ITEMS.

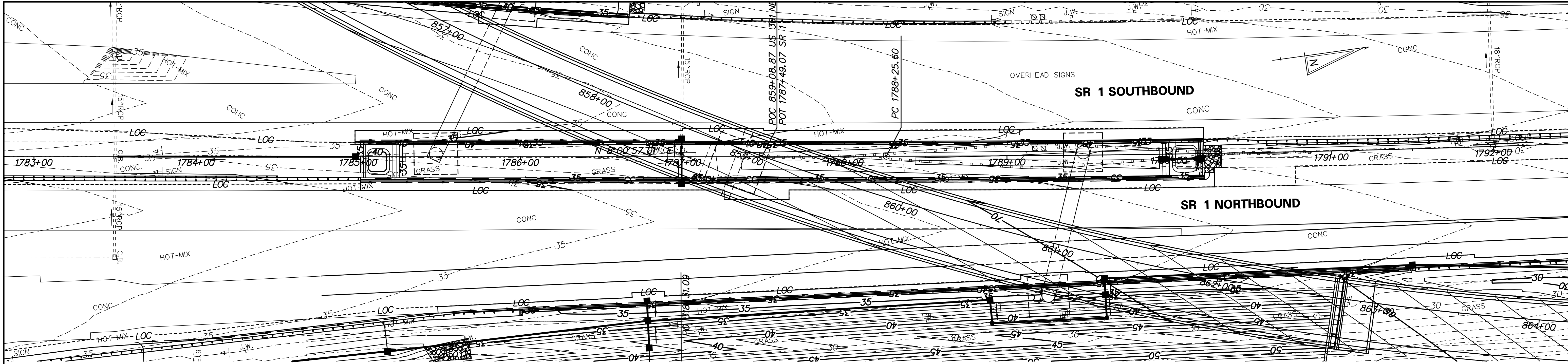
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ADDENDUMS / REVISIONS	
REVISED ITEM NO. AND ADDED DIMENSIONS:	
BRT/DMG	

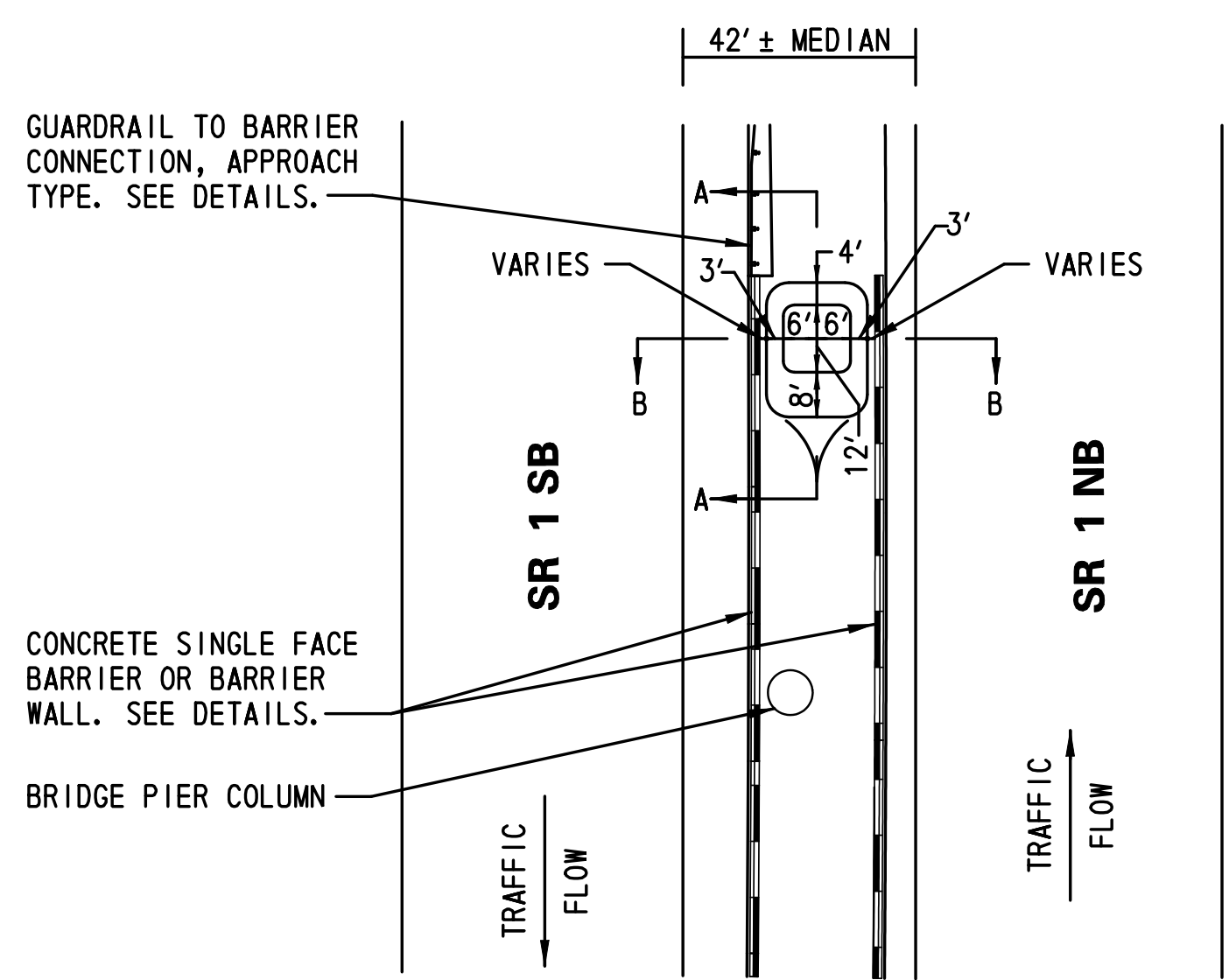


CONTRACT T200911302	BRIDGE NO.
COUNTY NEW CASTLE	DESIGNED BY: KAH
	CHECKED BY: BRT

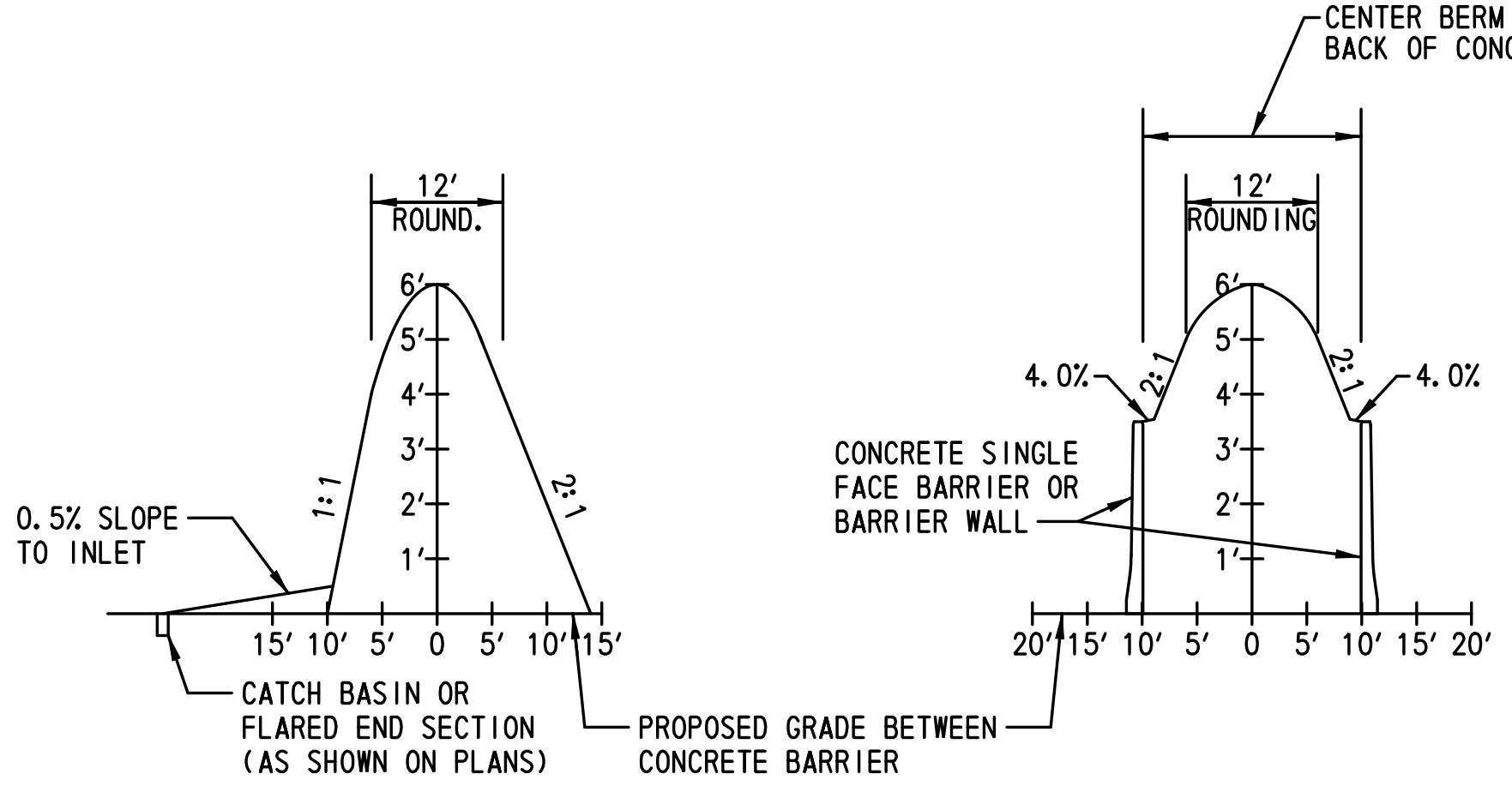




PLAN



NOTES:  
1. SEE PLAN SHEETS FOR LENGTH AND APPLICATION OF GUARDRAIL, CONCRETE SINGLE FACE BARRIER AND BARRIER WALLS.

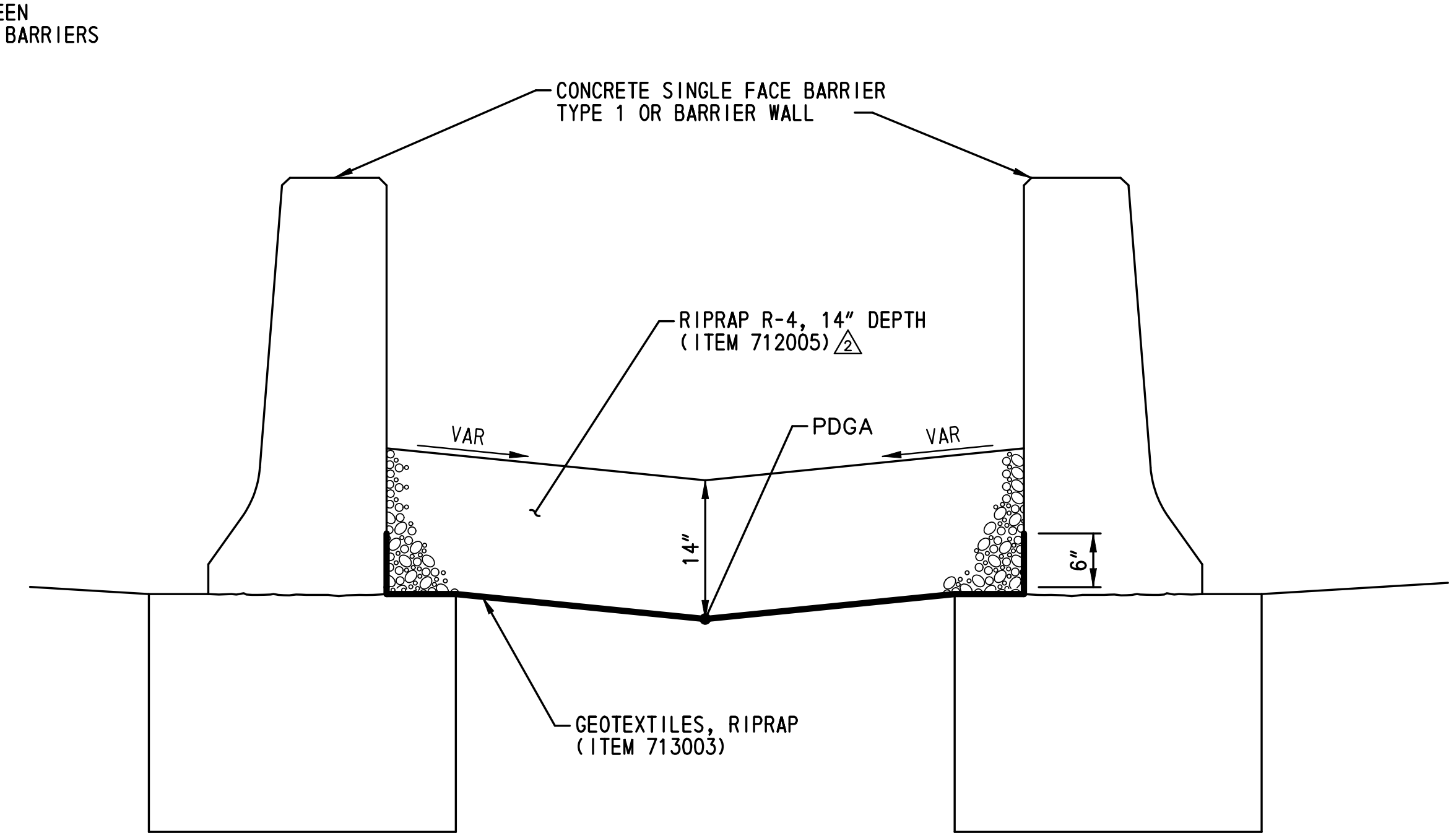


SECTION A-A  
NOT TO SCALE

SECTION B-B  
NOT TO SCALE

- NOTES:
- DISTANCE BETWEEN THE BACK OF BARRIER AND THE TOE OF SLOPE FOR THE EARTH MOUND SHALL NOT EXCEED FOUR (4) FEET.
  - SEE PLAN SHEETS FOR THE LOCATIONS OF EARTH MOUNDS AT BRIDGE PIER APPROACHES ALONG SR 1.
  - EARTH MOUND SHALL BE CONSTRUCTED OF MATERIALS MEETING THE REQUIREMENTS OF BORROW, TYPE F AND COMPACTED IN ACCORDANCE WITH SECTION 202.
  - STABILIZE MOUND SURFACE WITH TOPSOIL, SEED AND MULCH PER ROADWAY TYPICAL SECTIONS.

EARTH MOUND DETAILS AT  
BRIDGE PIER APPROACHES  
NOT TO SCALE



NOTES:  
1. SEE SHEETS TS-13, GG-03, AND GG-10 FOR APPLICATION.

RIPRAP R-4 MEDIAN TREATMENT  
NOT TO SCALE

**MEDIAN PIER PROTECTION  
BRIDGE BR1-3**

**BRIDGE PIER PROTECTION SYSTEM  
MAINLINE BRIDGE PIER AND  
MEDIAN OPENING PROTECTION PLAN  
NOT TO SCALE**



ADDENDUMS / REVISIONS	
REVISED ITEM NO.:	BRT/DMG



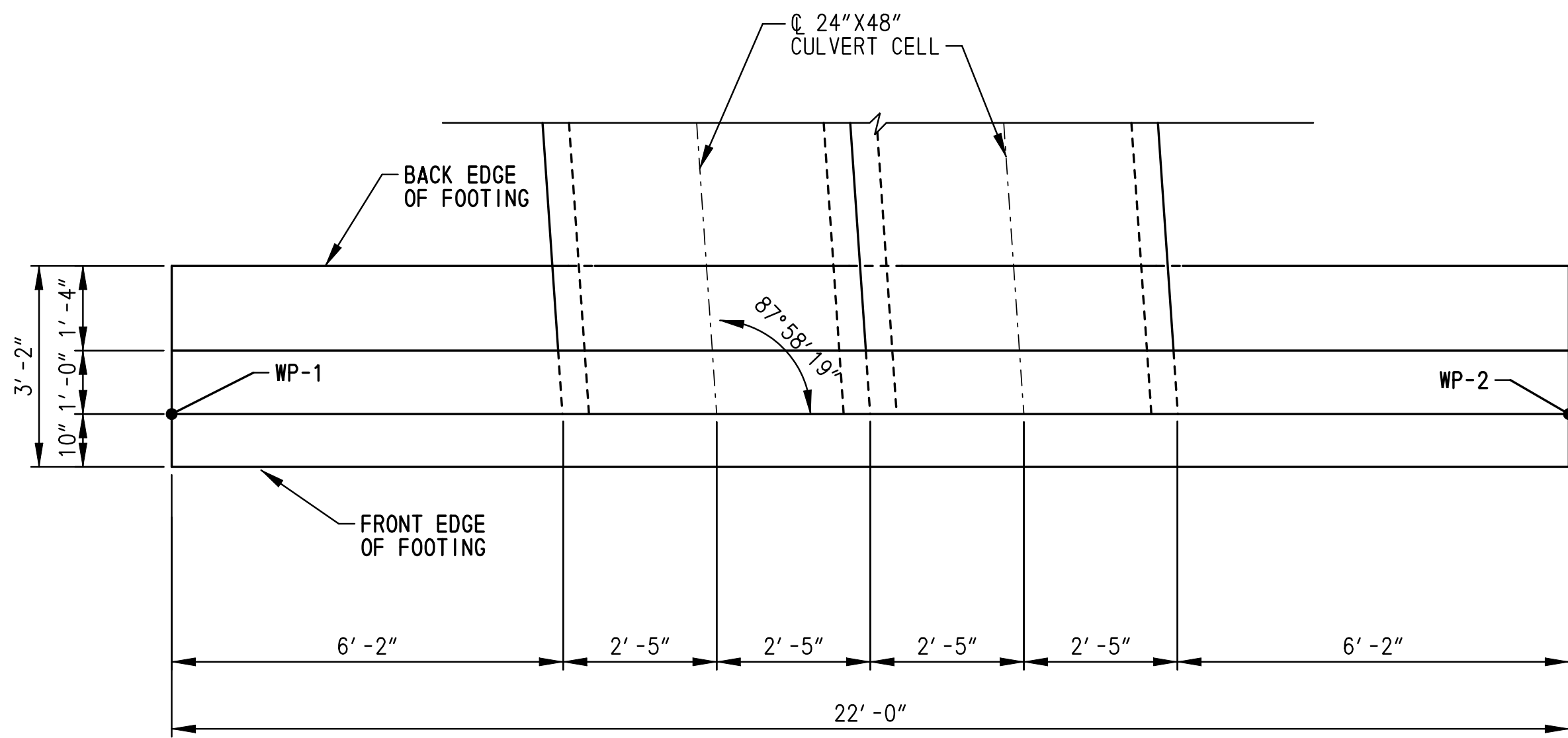
**US 301 &  
SR 1 INTERCHANGE**

CONTRACT	BRIDGE NO.
T200911302	
COUNTY	DESIGNED BY: KAH
NEW CASTLE	CHECKED BY: BRT

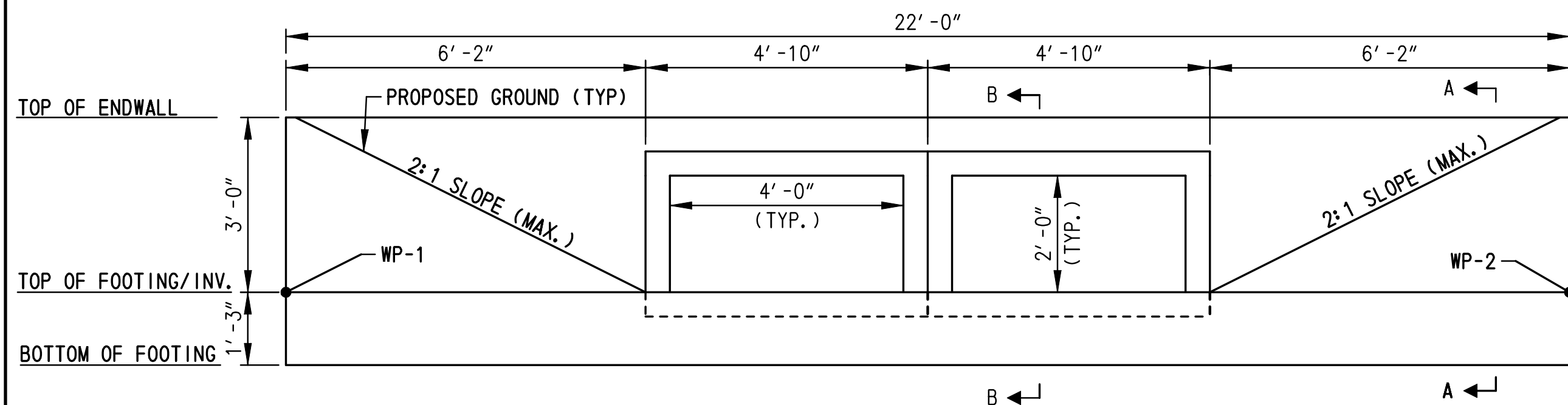
**CONSTRUCTION DETAILS**

DT-06
SHEET NO.
89
TOTAL SHTS.
491

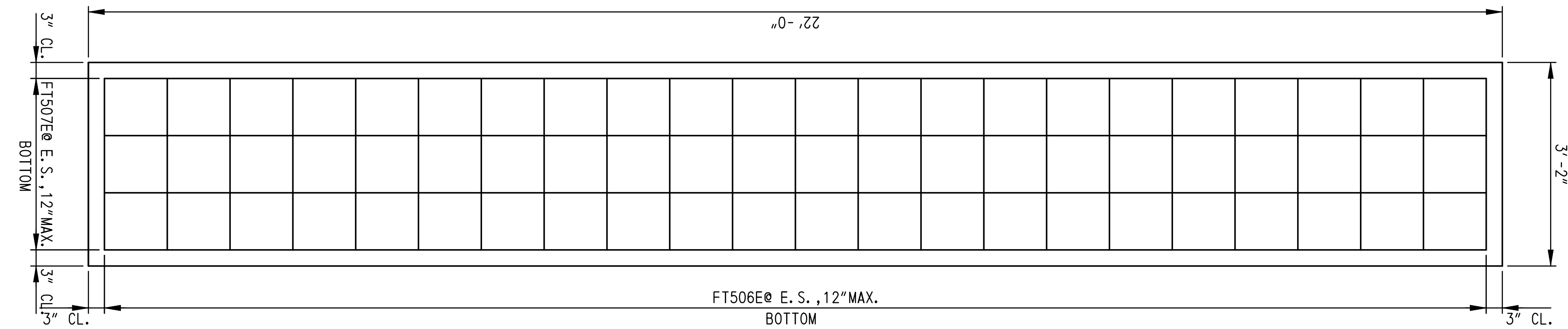
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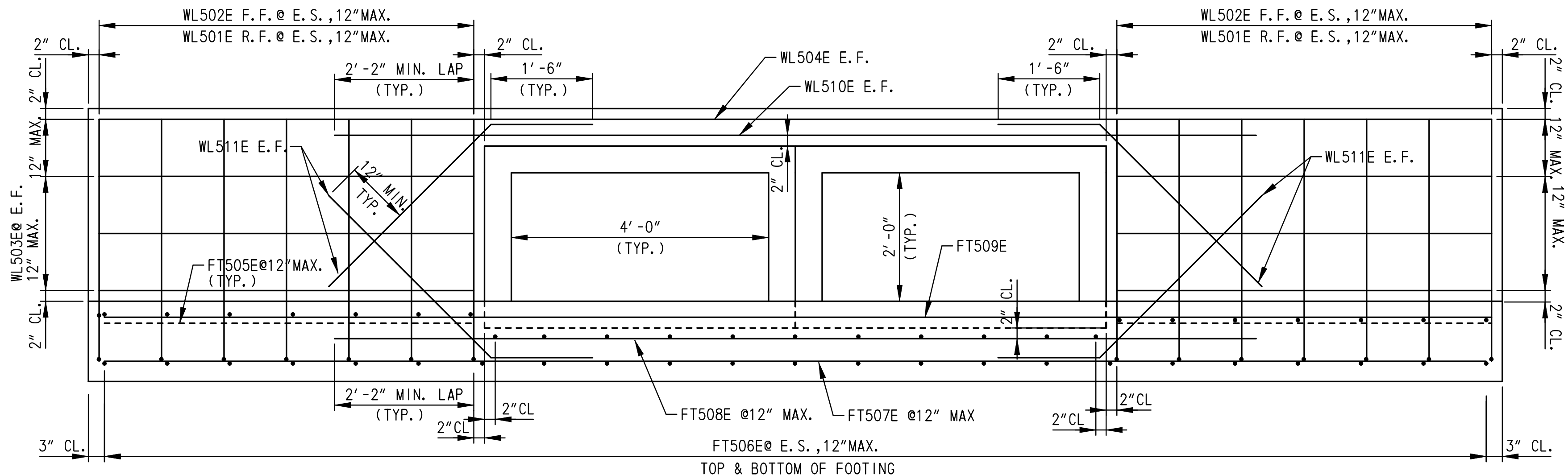
**ENDWALL NO. 404 - PLAN**  
SCALE: 1/2"=1'-0"



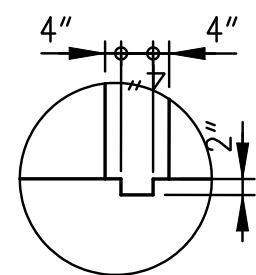
**ENDWALL NO. 404 - ELEVATION**  
SCALE: 1/2"=1'-0"



**ENDWALL NO. 404 - FOOTING REINFORCEMENT PLAN**  
SCALE: 3/4"=1'-0"



**ENDWALL NO. 404 - REINFORCEMENT ELEVATION**  
SCALE: 3/4"=1'-0"

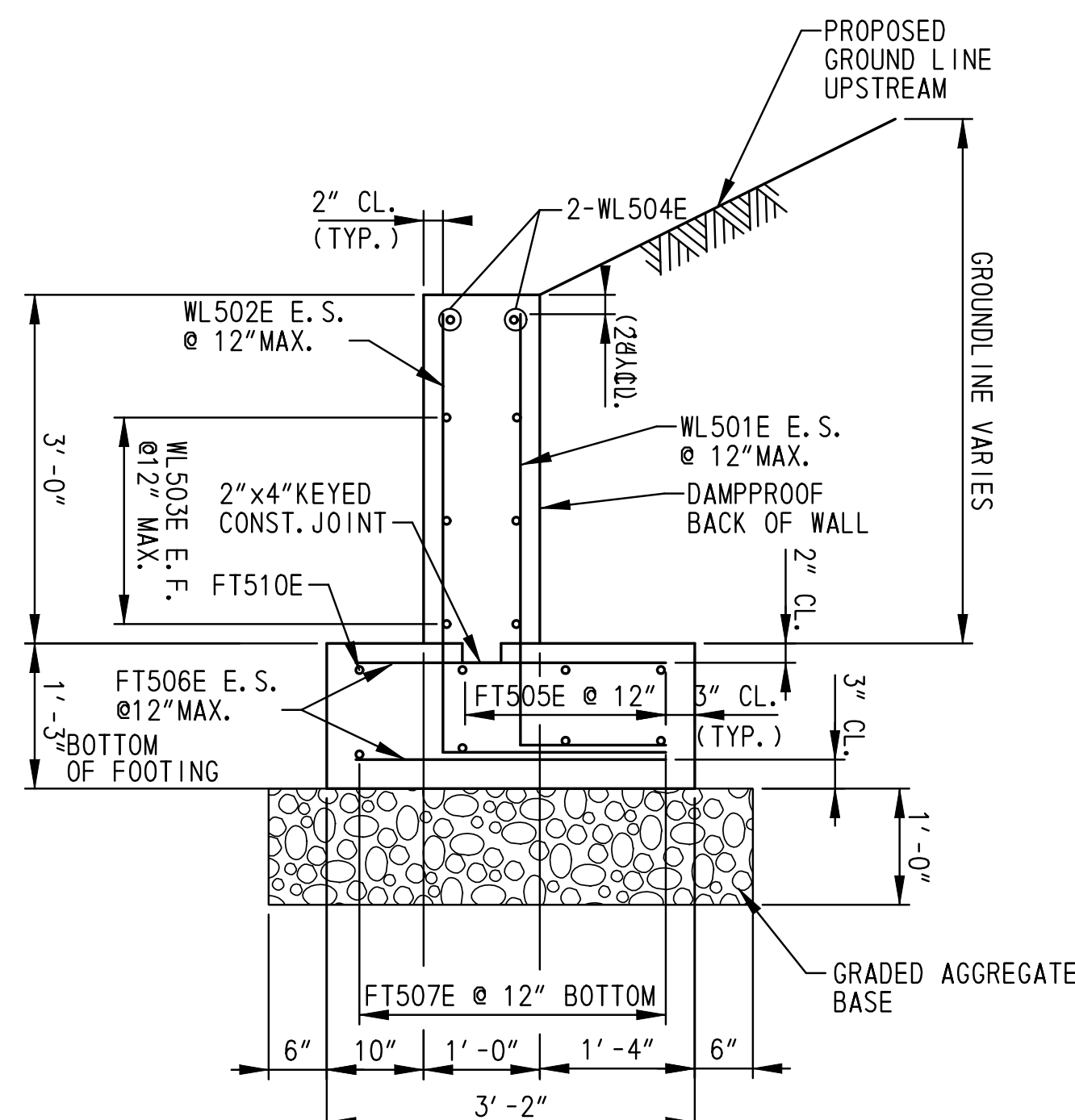


**CONSTRUCTION KEY DETAIL**  
SCALE: 1"=1'-0"

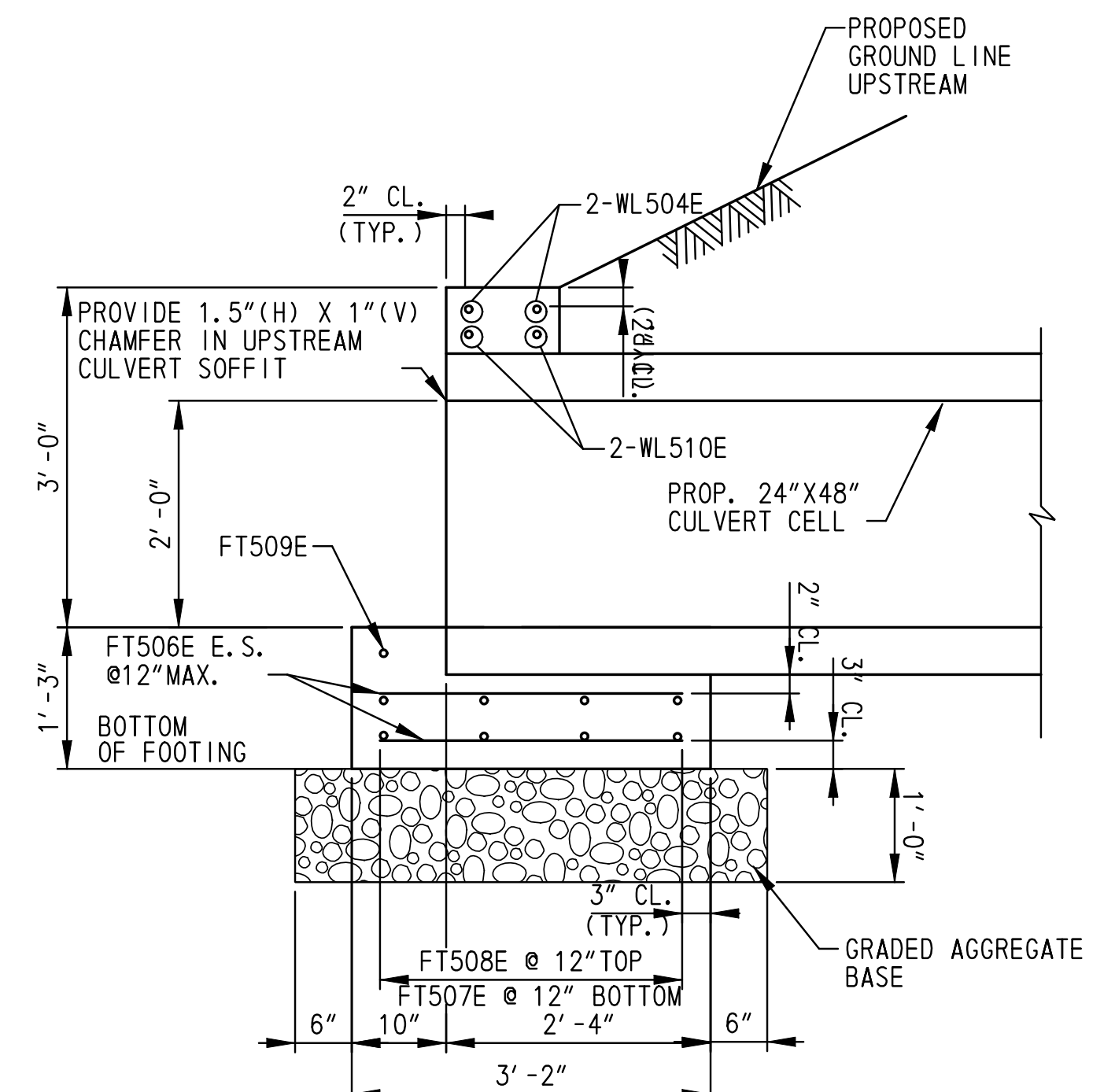
WORKING POINT LOCATION			
ENDWALL NO.	WP	NORTHING	EASTING
EW/404	WP-1	557813.93	590257.94
	WP-2	557792.38	590253.52

**ENDWALL NO. 404 NOTES:**

- MAXIMUM FACTORED BEARING RESISTANCE FOR THE ENDWALL IS 2 KSF.
- KEYS ARE NOMINAL SIZE.
- CONCRETE SHALL BE CLASS A, 4500 PSI.
- RIPRAP IN FRONT OF WALL NOT SHOWN FOR CLARITY.
- ENDWALL NO. 404 SHALL BE CAST-IN-PLACE. NO PRECAST ALTERNATIVE WILL BE ALLOWED.
- E. F. = EACH FACE  
F. F. = FRONT FACE  
R. F. = RARE FACE  
E. S. = EQUAL SPACING.
- SEE REINFORCING BAR LIST ON DRAWING DT-15.
- THE COST OF THE GRADED AGGREGATE BEDDING SHALL BE INCIDENTAL TO THE CONTRACT UNIT COST FOR THE ENDWALL (ITEM 617515).



**SECTION A-A**  
SCALE: 3/4"=1'-0"



**SECTION B-B**  
SCALE: 3/4"=1'-0"

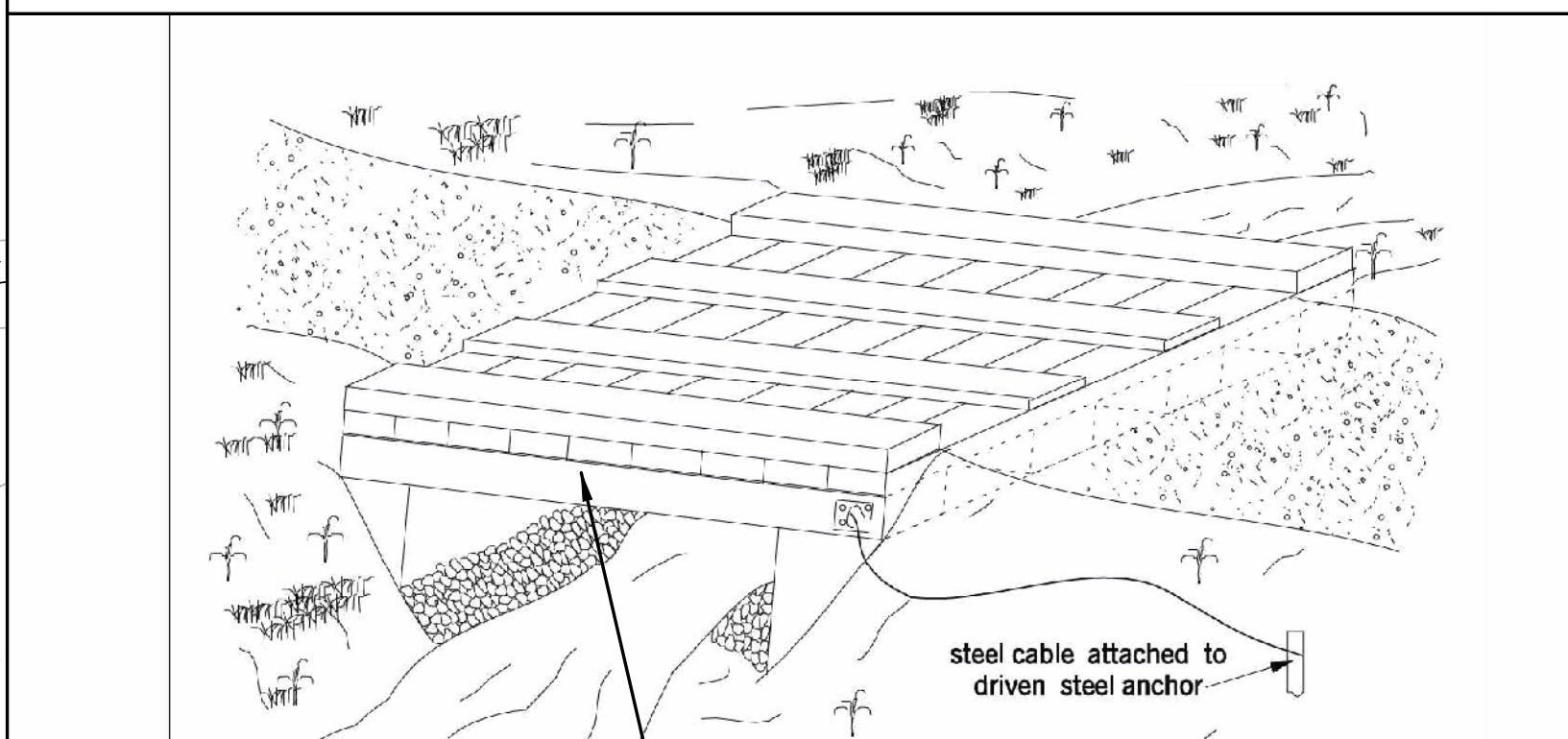
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ADDENDUMS / REVISIONS	
REVIS	REVISED ENDWALL NO., MODIFIED NOTES, AND MODIFIED WORKING POINTS: BRT/DMG

CONTRACT	BRIDGE NO.
T200911302	
COUNTY	DESIGNED BY: HQ
NEW CASTLE	CHECKED BY: JW



**DETAIL: TEMPORARY STREAM CROSSING FOR HAUL ROAD**



COST OF TEMPORARY CROSSING SHALL BE INCIDENTAL TO COST OF WETLAND ACCESS ROAD, TYPE II. WETLAND ACCESS ROAD, TYPE II AT THIS LOCATION SHALL BE PAID FOR AS SEPARATE ITEM NO. 202508.

**TEMPORARY STREAM CROSSING - CONSTRUCTION NOTES**

1. THE TEMPORARY CROSSING FOR HAUL ROAD ACTIVITIES SHALL BE CONSTRUCTED PER DETAIL AND NOTES. THE CONTRACTOR SHALL NOT ACCESS THE STREAM, IMPACT THE STREAM, NOR CHANGE THE STREAM PROFILE DURING CONSTRUCTION ACTIVITIES.
2. ABUTMENTS SHALL BE PLACED PARALLEL TO, AND ON, STABLE BANKS SUCH THAT THE BOTTOM OF THE STRUCTURE IS AT OR ABOVE MINIMUM TOP WIDTH ELEVATION PER CONTRACT SPECIAL PROVISIONS.
3. TEMPORARY ACCESS BRIDGE SHOULD BE CONSTRUCTED TO SPAN THE ENTIRE CHANNEL (OHW/WL). IF THE BANKFULL CHANNEL WIDTH EXCEEDS 8 FEET, THEN A FOOTING, PIER, OR OTHER SUPPORT MAY BE CONSTRUCTED WITHIN THE WATERWAY. NO SUPPORT WILL BE PERMITTED WITHIN THE CHANNEL FOR WATERWAYS LESS THAN 8 FEET WIDE. ONE ADDITIONAL BRIDGE SUPPORT WILL BE PERMITTED FOR EACH ADDITIONAL 8-FOOT WIDTH OF CHANNEL.
4. ALL DECKING MEMBERS SHOULD BE PLACED PERPENDICULARLY TO THE STRINGERS, BUTTED TIGHTLY, AND SECURELY FASTENED TO THE STRINGERS. DECKING MATERIALS MUST BE BUTTED TIGHTLY TO PREVENT ANY SOIL MATERIAL TRACKED ONTO THE BRIDGE FROM FALLING INTO THE WATERWAY.
5. ALTHOUGH RUN PLANKS ARE OPTIONAL, THEY MAY BE NECESSARY TO PROPERLY DISTRIBUTE LOADS. ONE RUN PLANK SHOULD BE PROVIDED FOR EACH TRACK OF THE EQUIPMENT WHEELS AND SHOULD BE SECURELY FASTENED TO THE LENGTH OF THE SPAN.
6. CURBS OR FENDERS MAY BE INSTALLED ALONG THE OUTER SIDES OF THE DECK TO PROVIDE ADDITIONAL SAFETY.
7. BRIDGE SHOULD BE SECURELY ANCHORED AT ONE END USING A STEEL CABLE OR CHAIN TO PREVENT THE BRIDGE FROM FLOATING DOWNSTREAM AND POSSIBLY CAUSING AN OBSTRUCTION TO THE FLOW. ANCHORING AT ONLY ONE END WILL PREVENT CHANNEL OBSTRUCTION IN THE EVENT THAT FLOOD WATERS FLOAT THE BRIDGE. ACCEPTABLE ANCHORS ARE LARGE TREES, BOULDERS, OR DRIVEN STEEL ANCHORS.
8. ALL AREAS DISTURBED DURING INSTALLATION SHALL BE STABILIZED WITHIN 14 CALENDAR DAYS OF REMOVAL.
9. INSPECTION AND MAINTENANCE SHALL BE PERFORMED ON A DAILY BASIS AND AFTER ALL STORM EVENTS TO ENSURE THAT THE STRUCTURE COMPLIES WITH ALL STANDARDS AND SPECIFICATIONS. THIS INCLUDES BRIDGE AND STREAMBANK STABILITY AND REMOVAL OF TRAPPED SEDIMENT AND DEBRIS WHICH SHALL BE DISPOSED OF IN A STABILIZED AREA OUTSIDE OF THE FLOODPLAIN.
10. REMOVAL OF THE TEMPORARY BRIDGE SHALL INCLUDE ALL BRIDGE MATERIALS AND STRUCTURES WITHIN 14 CALENDAR DAYS OF START OF DEMOLITION. AREAS DISTURBED IN THE PROCESS SHALL BE PERMANENTLY STABILIZED PER NOTE THIS SHEET AND PER ENVIRONMENTAL COMPLIANCE GUIDELINES OF THE CONTRACT DOCUMENTS.

**NOTES:**

1. REINFORCED SILT FENCE SHALL BE INSTALLED ALONG BOTH SIDES OF THE PROPOSED HAUL ROAD AND THE PROPOSED ACCESS ROAD IN 200-FOOT LENGTHS WITH THE DOWNSTREAM-MOST 10-FOOT OF EACH LENGTH TURNED UP SLOPE TOWARD THE ROAD.
2. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED FOR INGRESS AND EGRESS AT HYEYTS CORNER ROAD.
3. THE COST OF INSTALLATION AND MAINTENANCE ALL DEVICES FOR EROSION AND SEDIMENT CONTROL, MAINTENANCE OF STREAMFLOW, AND TEMPORARY CROSSING OF STREAM SHALL BE INCIDENTAL TO THE COST OF THE WETLAND ACCESS ROAD, TYPE II FOR HAUL ROAD UNLESS OTHERWISE NOTED BY NOTE 41 OF THE GENERAL NOTES ON PN-02.

CONTRACTOR SHALL COORDINATE PROPOSED ACCESS ROAD CONSTRUCTION WITH ADJUSTMENT OF CONTRACT 1A EROSION AND SEDIMENT CONTROLS.

BORROW SITE VILLAGE OF SCOTT RUN EAST - NO MATCHLINE INDICATED INFORMATION FOR EROSION AND SEDIMENT CONTROL DEVICES SHOWN HEREON ARE LOCATED ON THE BORROW SITE GRADING PLAN FOR VILLAGE OF SCOTT RUN EAST, SHEET GR-01. CONSTRUCTION ACTIVITIES ON THE GRADING SHEET SHALL ALSO BE COORDINATED WITH THIS SHEET.

**LIMIT CONTRACT T200911302**

**LIMIT CONTRACT T200911302**

**MATCH LINE LL - SEE DWG. CS-102**

**MATCH LINE STA 848+00 - SEE DWG. CS-102**

SEE NOTES 1 AND 2 FOR HAUL ROAD EROSION AND SEDIMENT CONTROL REQUIREMENTS

SEE NOTE 2

WETLAND ACCESS ROAD

CONTRACTOR SHALL CONSTRUCT A TEMPORARY CROSSING OF STREAM ALONG HAUL ROAD, PER CONTRACT SPECIAL PROVISIONS FOR WETLAND ACCESS ROAD, TYPE II. SEE MINIMUM SIZING CRITERION THIS SHEET.

SEE NOTES 1 AND 2 FOR HAUL ROAD EROSION AND SEDIMENT CONTROL REQUIREMENTS

**SIZING OF HYDRAULIC OPENING OF TEMPORARY CROSSING**

MINIMUM AREA REQUIRED: 40.0 SF  
MINIMUM TOP WIDTH ELEVATION: 28.0

ACCESS ROAD	
TYPE	S. Y.
WETLAND ACCESS ROAD	425
COMPUTED FLOWRATE	
STORM EVENT	CFS
2-YEAR	100 +/-

**CONSTRUCTION PHASING HAUL ROAD**

CS-101
SHEET NO.
380
TOTAL SHTS.
491

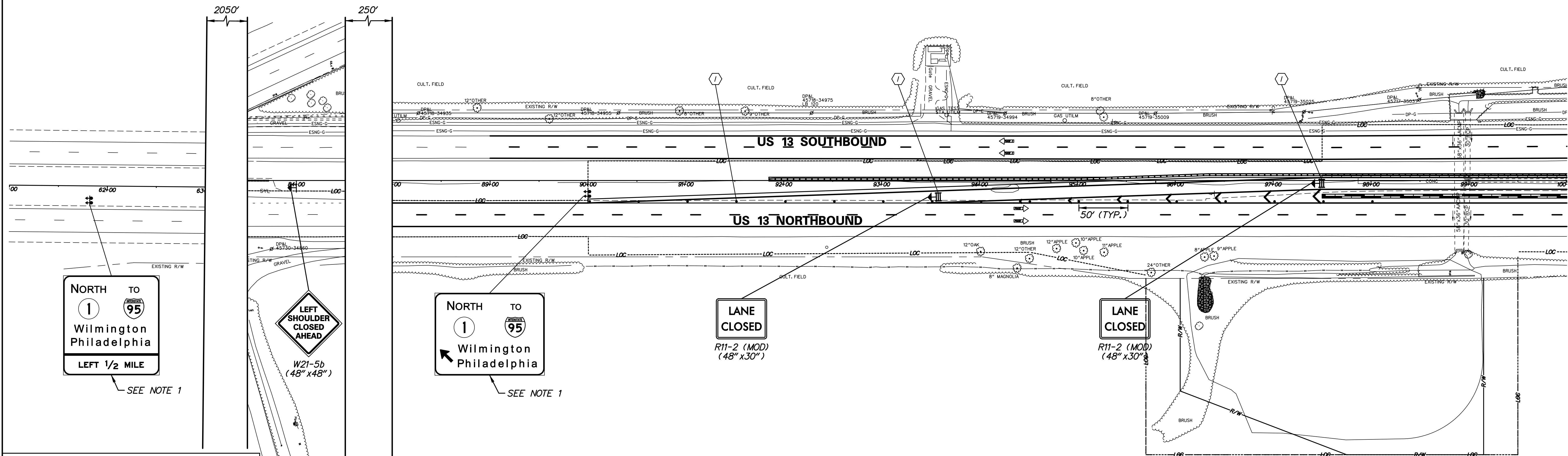


SEQUENCE OF CONSTRUCTION

- ① THE CONTRACTOR T200911302 CONTRACTOR SHALL MAINTAIN THE TEMPORARY TRAFFIC CONTROL DEVICES AND SIGN COVERINGS SHOWN ON THIS PLAN INSTALLED BY THE CONTRACT T201011302 CONTRACTOR FOR THE TEMPORARY CLOSURE OF THE US 13 RAMP TO SR1 NORTHBOUND (RAMP R).
- ② THE CONTRACT T200911302 CONTRACTOR SHALL REMOVE ALL TEMPORARY TRAFFIC CONTROL DEVICES AND SIGN COVERINGS SHOWN ON THIS PLAN AND OPEN THE US 13 RAMP TO SR 1 NORTHBOUND (RAMP R) UPON STARTING CONSTRUCTION PHASE 3 OF THIS CONTRACT.

NOTES:

- 1. CONTRACT T201011302 CONTRACTOR SHALL COVER SIGN WITH OPAQUE MATERIAL AFTER INSTALLATION OF SIGN AND LEAVE SIGN COVERED AT COMPLETION OF CONTRACT. CONTRACT T200911302 CONTRACTOR SHALL REMOVE SIGN COVERING UPON OPENING OF RAMP R IN THAT CONTRACT.
- 2. AT THE COMPLETION OF CONSTRUCTION THE CONTRACT T201011302 CONTRACTOR SHALL FURNISH AND INSTALL TEMPORARY TRAFFIC CONTROL DEVICES TO REMAIN AS SHOWN ON THIS PLAN. THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE AND SERVE TO TEMPORARILY CLOSE RAMP R UNTIL ITS OPENING BY THE CONTRACT T200911302 CONTRACTOR. MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES DURING THE INTERIM CLOSURE PERIOD AND UPON OPENING OF RAMP R SHALL BE THE RESPONSIBILITY OF THE CONTRACT T200911302 CONTRACTOR.
- 3. PAYMENT FOR MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THIS PLAN BY THE CONTRACT T200911302 CONTRACTOR WILL NOT BE MEASURED AND PAID FOR BUT WILL BE INCIDENTAL TO ITEM 743000 - MAINTENANCE OF TRAFFIC.

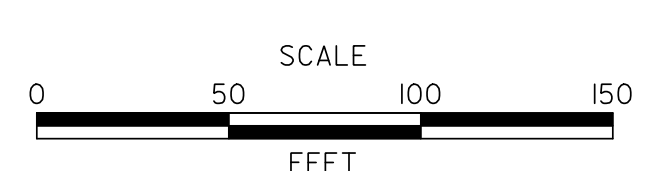


CONSTRUCTION PHASING LEGEND

- PROPOSED CONSTRUCTION THIS PHASE
- ▨ TEMPORARY CONSTRUCTION
- ⇄ EXISTING SIGN/PROPOSED TEMPORARY SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- ⇄ TRAFFIC FLOW ARROW
- ⓐ 4\"/>

ADDENDUMS / REVISIONS

△	REVISED ITEM NO.: BRT/DMG



US 301 & SR 1 INTERCHANGE

CONTRACT	T200911302	BRIDGE NO.	
COUNTY	NEW CASTLE	DESIGNED BY:	J.D.C.
		CHECKED BY:	J.M.M.

CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN - PHASE 3

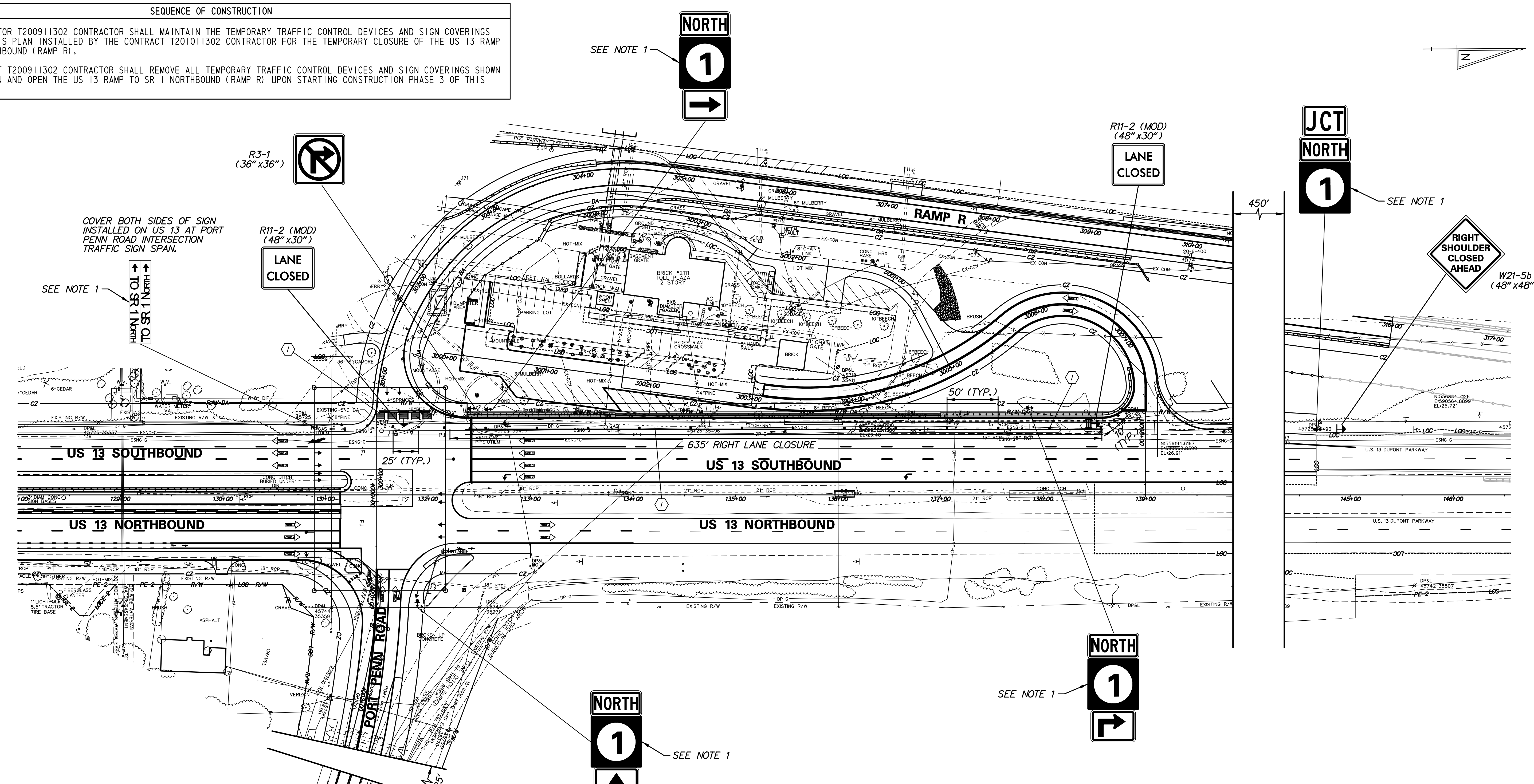
CS-312
SHEET NO.
419
TOTAL SHTS.
491

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SEQUENCE OF CONSTRUCTION

- THE CONTRACTOR T200911302 CONTRACTOR SHALL MAINTAIN THE TEMPORARY TRAFFIC CONTROL DEVICES AND SIGN COVERINGS SHOWN ON THIS PLAN INSTALLED BY THE CONTRACT T201011302 CONTRACTOR FOR THE TEMPORARY CLOSURE OF THE US 13 RAMP TO SR1 NORTHBOUND (RAMP R).
- THE CONTRACT T200911302 CONTRACTOR SHALL REMOVE ALL TEMPORARY TRAFFIC CONTROL DEVICES AND SIGN COVERINGS SHOWN ON THIS PLAN AND OPEN THE US 13 RAMP TO SR1 NORTHBOUND (RAMP R) UPON STARTING CONSTRUCTION PHASE 3 OF THIS CONTRACT.



COVER BOTH SIDES OF SIGN INSTALLED ON US 13 AT PORT PENN ROAD INTERSECTION TRAFFIC SIGN SPAN.

SEE NOTE 1  
 ALTERNATE TO SR 1 NORTH

R11-2 (MOD)  
 (48" x 30")  
 LANE CLOSED

W21-5b  
 (48" x 48")  
 RIGHT SHOULDER CLOSED AHEAD

CONSTRUCTION PHASING LEGEND

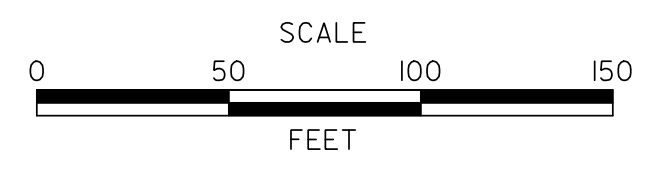
- PROPOSED CONSTRUCTION THIS PHASE
- TEMPORARY CONSTRUCTION
- EXISTING SIGN/PROPOSED TEMPORARY SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- TRAFFIC FLOW ARROW
- 4" SOLID WHITE PAINT
- 4" SOLID YELLOW PAINT
- 4" BROKEN WHITE PAINT (10' LINE, 30' GAP)
- 4" DOTTED WHITE PAINT (3' LINE, 9' GAP)
- WHITE PAINT SYMBOL/LEGEND
- PORTABLE P.C.C. SAFETY BARRIER (F SHAPE)
- PORTABLE IMPACT ATTENUATOR
- ARROW PANEL
- REMOVE EXISTING STRIPING BY CONTRACTOR

NOTES:

- CONTRACT T201011302 CONTRACTOR SHALL COVER SIGN WITH OPAQUE MATERIAL AFTER INSTALLATION OF SIGN AND LEAVE SIGN COVERED AT COMPLETION OF CONTRACT. CONTRACT T200911302 CONTRACTOR SHALL REMOVE SIGN COVERING UPON OPENING OF RAMP R IN THAT CONTRACT.
- AT THE COMPLETION OF CONSTRUCTION THE CONTRACT T201011302 CONTRACTOR SHALL FURNISH AND INSTALL TEMPORARY TRAFFIC CONTROL DEVICES TO REMAIN AS SHOWN ON THIS PLAN. THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE AND SERVE TO TEMPORARILY CLOSE RAMP R UNTIL ITS OPENING BY THE CONTRACT T200911302 CONTRACTOR. MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES DURING THE INTERIM CLOSURE PERIOD AND UPON OPENING OF RAMP R SHALL BE THE RESPONSIBILITY OF THE CONTRACT T200911302 CONTRACTOR.
- PAYMENT FOR MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THIS PLAN BY THE CONTRACT T200911302 CONTRACTOR WILL NOT BE MEASURED AND PAID FOR BUT WILL BE INCIDENTAL TO ITEM 743000 - MAINTENANCE OF TRAFFIC.

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ADDENDUMS / REVISIONS
REVISED ITEM NO.: BRT/DMG



**US 301 & SR 1 INTERCHANGE**

CONTRACT	BRIDGE NO.
T200911302	
COUNTY	DESIGNED BY: J.D.C.
NEW CASTLE	CHECKED BY: J.M.M.

**CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN - PHASE 3**

CS-313
SHEET NO.
420
TOTAL SHTS.
491

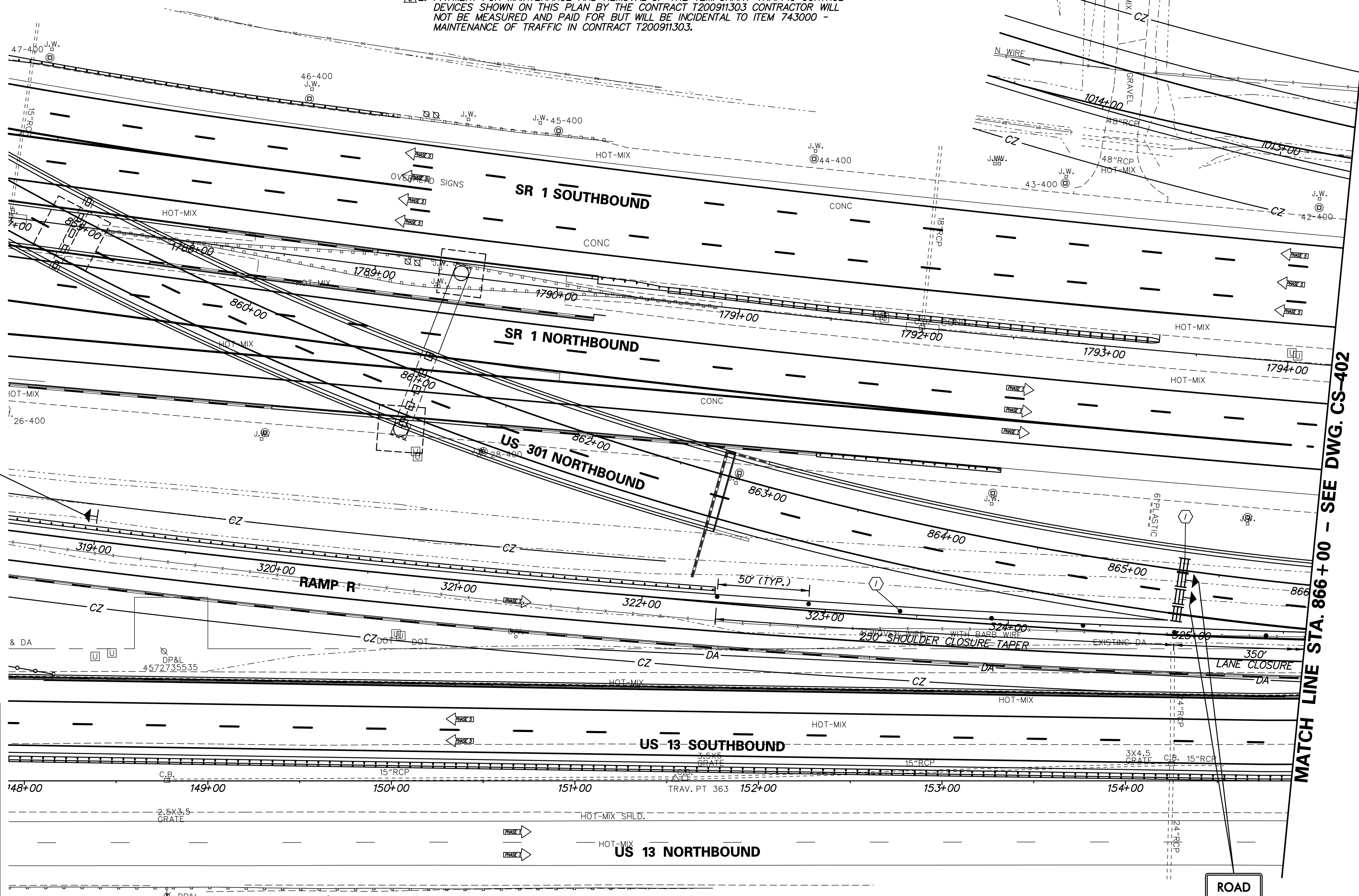
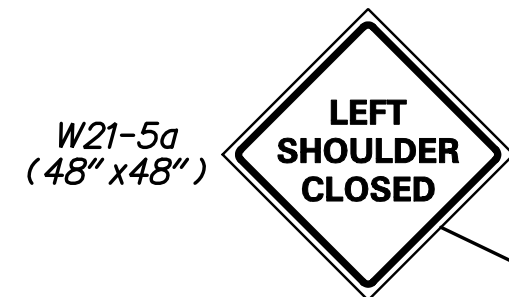


SEQUENCE OF CONSTRUCTION - PHASE 4

1. UPON COMPLETION OF ALL CONSTRUCTION ACTIVITIES, REMOVE ALL PHASE 3 TEMPORARY TRAFFIC CONTROL DEVICES EXCEPT THOSE NEEDED FOR THE CLOSURE OF THE US 301 NORTHBOUND ROADWAY SOUTH OF STA. 865+00 AND THE SR 1 SOUTHBOUND RAMP TO US 301 AS SHOWN ON THIS PLAN. THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THESE PHASE 4 DRAWINGS SHALL REMAIN UNTIL OPENING OF THE US 301 ROADWAY BY OTHERS (SEE NOTES 1 & 2).

NOTES:

- AT THE COMPLETION OF CONSTRUCTION THE CONTRACTOR SHALL FURNISH AND INSTALL TEMPORARY TRAFFIC CONTROL DEVICES TO REMAIN AS SHOWN ON DRAWINGS CS-401 THROUGH CS-407 (PHASE 4). THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE AND SERVE TO TEMPORARILY CLOSE THE NEW US 301 ROADWAY UNTIL ITS OFFICIAL OPENING. MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES DURING THE INTERIM CLOSURE PERIOD AND UPON OPENING OF THE US 301 ROADWAY SHALL BE THE RESPONSIBILITY OF OTHERS (CONTRACT T200911303 CONTRACTOR).
- PAYMENT FOR MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THIS PLAN BY THE CONTRACT T200911303 CONTRACTOR WILL NOT BE MEASURED AND PAID FOR BUT WILL BE INCIDENTAL TO ITEM 743000 - MAINTENANCE OF TRAFFIC IN CONTRACT T200911303.



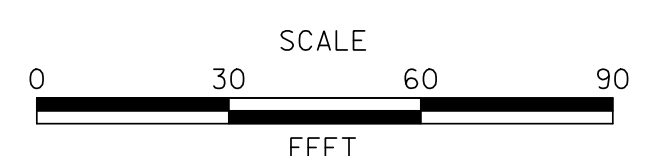
MATCH LINE STA. 866+00 - SEE DWG. CS-402

CONSTRUCTION PHASING LEGEND

- TEMPORARY CONSTRUCTION
- PROPOSED CONSTRUCTION THIS PHASE
- EXISTING SIGN/PROPOSED TEMPORARY SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- TRAFFIC FLOW ARROW
- 4" SOLID WHITE PAINT
- 4" SOLID YELLOW PAINT
- 4" BROKEN WHITE PAINT (10' LINE, 30' GAP)
- 4" DOTTED WHITE PAINT (3' LINE, 9' GAP)
- 10" SOLID WHITE PAINT
- WHITE PAINT SYMBOL/LEGEND
- PORTABLE P.C.C. SAFETY BARRIER (F SHAPE)
- PORTABLE IMPACT ATTENUATOR
- ARROW PANEL
- REMOVE EXISTING STRIPING BY CONTRACTOR

ADDENDUMS / REVISIONS

REVISED ITEM NO.	DESCRIPTION
1	BRT/DMG



US 301 & SR 1 INTERCHANGE

CONTRACT	T200911302
COUNTY	NEW CASTLE
BRIDGE NO.	BR1-433, RW1-3 & RW1-3R
DESIGNED BY:	J.D.C.
CHECKED BY:	J.M.M.

CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN - PHASE 4

CS-401	
SHEET NO.	421
TOTAL SHTS.	491

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SEQUENCE OF CONSTRUCTION - PHASE 4

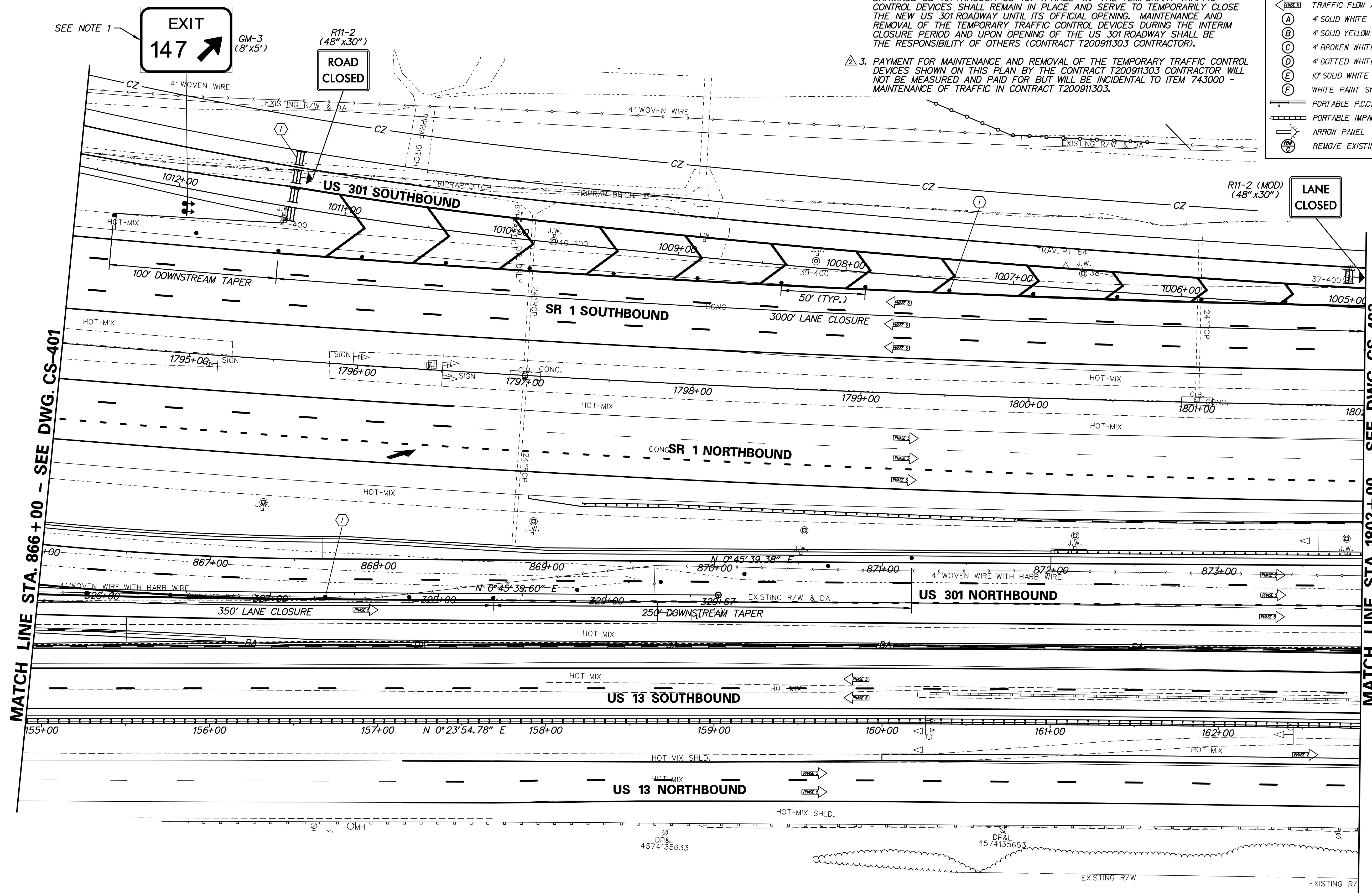
UPON COMPLETION OF ALL CONSTRUCTION ACTIVITIES, REMOVE ALL PHASE 3 TEMPORARY TRAFFIC CONTROL DEVICES EXCEPT THOSE NEEDED FOR THE CLOSURE OF THE US 301 NORTHBOUND ROADWAY SOUTH OF STA. 865+00 AND THE SR 1 SOUTHBOUND RAMP TO US 301 AS SHOWN ON THIS PLAN. THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THESE PHASE 4 DRAWINGS SHALL REMAIN UNTIL OPENING OF THE US 301 ROADWAY BY OTHERS (SEE NOTES 1, 2 & 3).

NOTES:

- CONTRACT T200911302 CONTRACTOR SHALL COVER SIGN WITH OPAQUE MATERIAL AFTER INSTALLATION OF SIGN AND LEAVE SIGN COVERED AT COMPLETION OF CONTRACT. CONTRACT T200911303 CONTRACTOR SHALL REMOVE SIGN COVERING UPON OPENING OF US 301 ROADWAY.
- AT THE COMPLETION OF CONSTRUCTION THE CONTRACTOR SHALL FURNISH AND INSTALL TEMPORARY TRAFFIC CONTROL DEVICES TO REMAIN AS SHOWN ON DRAWINGS CS-401 THROUGH CS-407 (PHASE 4). THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE AND SERVE TO TEMPORARILY CLOSE THE NEW US 301 ROADWAY UNTIL ITS OFFICIAL OPENING. MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES DURING THE INTERIM CLOSURE PERIOD AND UPON OPENING OF THE US 301 ROADWAY SHALL BE THE RESPONSIBILITY OF OTHERS (CONTRACT T200911303 CONTRACTOR).
- PAYMENT FOR MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THIS PLAN BY THE CONTRACT T200911303 CONTRACTOR WILL NOT BE MEASURED AND PAID FOR BUT WILL BE INCIDENTAL TO ITEM 743000 - MAINTENANCE OF TRAFFIC IN CONTRACT T200911303.

CONSTRUCTION PHASING LEGEND

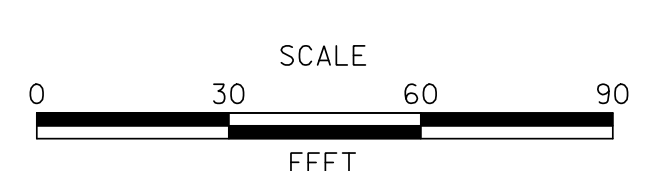
- TEMPORARY CONSTRUCTION
- PROPOSED CONSTRUCTION THIS PHASE
- EXISTING SIGN/PROPOSED TEMPORARY SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- TRAFFIC FLOW ARROW
- 4" SOLID WHITE PAINT
- 4" SOLID YELLOW PAINT
- 4" BROKEN WHITE PAINT (10' LINE, 30' GAP)
- 4" DOTTED WHITE PAINT (3' LINE, 9' GAP)
- 10" SOLID WHITE PAINT
- WHITE PAINT SYMBOL/LEGEND
- PORTABLE P.C.C. SAFETY BARRIER (F SHAPE)
- PORTABLE IMPACT ATTENUATOR
- ARROW PANEL
- REMOVE EXISTING STRIPING BY CONTRACTOR



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ADDENDUMS / REVISIONS	
REVISED ITEM NO.:	BRT/DMG



**US 301 & SR 1 INTERCHANGE**

CONTRACT	T200911302	BRIDGE NO.	RW1-3 & RW1-3R
COUNTY	NEW CASTLE	DESIGNED BY:	J.D.C.
		CHECKED BY:	J.M.M.

**CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN - PHASE 4**

CS-402	
SHEET NO.	422
TOTAL SHTS.	491

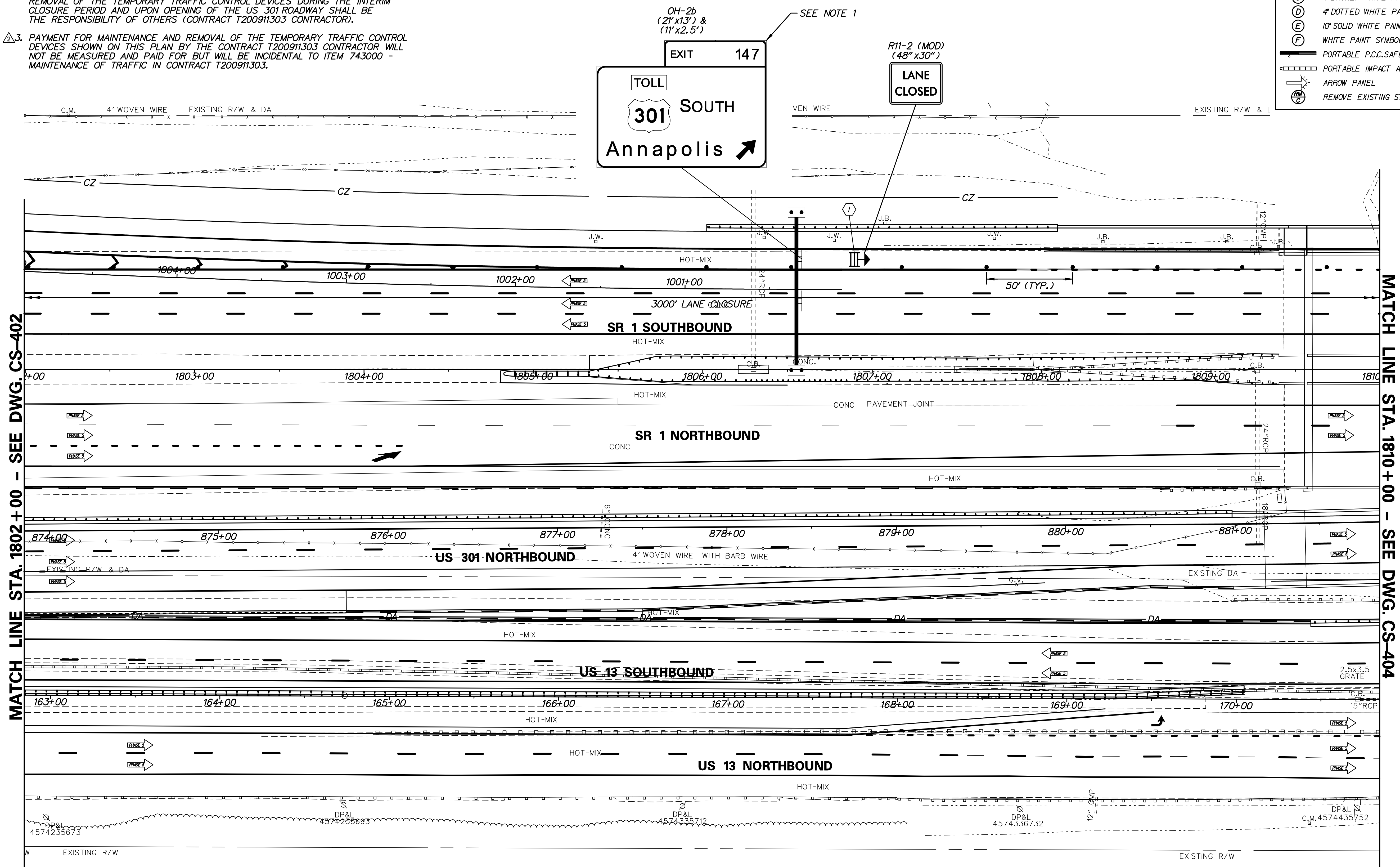
**NOTES:**

- CONTRACT T200911302 CONTRACTOR SHALL COVER SIGN WITH OPAQUE MATERIAL AFTER INSTALLATION OF SIGN AND LEAVE SIGN COVERED AT COMPLETION OF CONTRACT. CONTRACT T200911303 CONTRACTOR SHALL REMOVE SIGN COVERING UPON OPENING OF US 301 ROADWAY.
- AT THE COMPLETION OF CONSTRUCTION THE CONTRACTOR SHALL FURNISH AND INSTALL TEMPORARY TRAFFIC CONTROL DEVICES TO REMAIN AS SHOWN ON DRAWINGS CS-401 THROUGH CS-407 (PHASE 4). THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE AND SERVE TO TEMPORARILY CLOSE THE NEW US 301 ROADWAY UNTIL ITS OFFICIAL OPENING. MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES DURING THE INTERIM CLOSURE PERIOD AND UPON OPENING OF THE US 301 ROADWAY SHALL BE THE RESPONSIBILITY OF OTHERS (CONTRACT T200911303 CONTRACTOR).
- PAYMENT FOR MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THIS PLAN BY THE CONTRACT T200911303 CONTRACTOR WILL NOT BE MEASURED AND PAID FOR BUT WILL BE INCIDENTAL TO ITEM 743000 - MAINTENANCE OF TRAFFIC IN CONTRACT T200911303.

**SEQUENCE OF CONSTRUCTION - PHASE 4**  
 (I) UPON COMPLETION OF ALL CONSTRUCTION ACTIVITIES, REMOVE ALL PHASE 3 TEMPORARY TRAFFIC CONTROL DEVICES EXCEPT THOSE NEEDED FOR THE CLOSURE OF THE US 301 NORTHBOUND ROADWAY SOUTH OF STA. 865+00 AND THE SR 1 SOUTHBOUND RAMP TO US 301 AS SHOWN ON THIS PLAN. THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THESE PHASE 4 DRAWINGS SHALL REMAIN UNTIL OPENING OF THE US 301 ROADWAY BY OTHERS (SEE NOTES 1, 2 & 3).

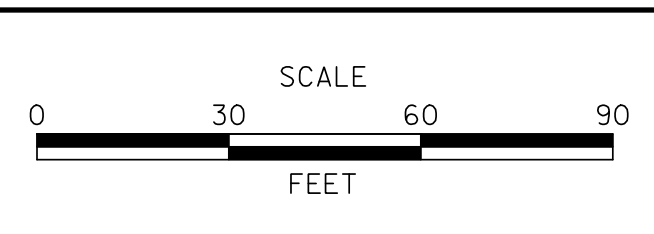
**CONSTRUCTION PHASING LEGEND**

	TEMPORARY CONSTRUCTION
	PROPOSED CONSTRUCTION THIS PHASE
	EXISTING SIGN/PROPOSED TEMPORARY SIGN
	TYPE III BARRICADE
	PLASTIC DRUM
	TRAFFIC FLOW ARROW
	4" SOLID WHITE PAINT
	4" SOLID YELLOW PAINT
	4" BROKEN WHITE PAINT (10' LINE, 30' GAP)
	4" DOTTED WHITE PAINT (3' LINE, 9' GAP)
	10" SOLID WHITE PAINT
	WHITE PAINT SYMBOL/LEGEND
	PORTABLE P.C.C. SAFETY BARRIER (F SHAPE)
	PORTABLE IMPACT ATTENUATOR
	ARROW PANEL
	REMOVE EXISTING STRIPING BY CONTRACTOR



**ADDENDUMS / REVISIONS**

REVISED ITEM NO.:	BRT/DMG



**US 301 & SR 1 INTERCHANGE**

CONTRACT	T200911302
COUNTY	NEW CASTLE
BRIDGE NO.	BRI-903N, BRI-903S, BRI-432 & RWI-3R
DESIGNED BY:	J.D.C.
CHECKED BY:	J.M.M.

**CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN - PHASE 4**

SHEET NO.	423
TOTAL SHTS.	491

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**CONSTRUCTION PHASING LEGEND**

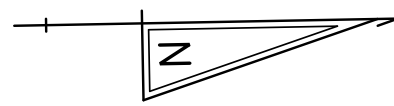
- TEMPORARY CONSTRUCTION
- PROPOSED CONSTRUCTION THIS PHASE
- EXISTING SIGN/PROPOSED TEMPORARY SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- TRAFFIC FLOW ARROW
- 4" SOLID WHITE PAINT
- 4" SOLID YELLOW PAINT
- 4" BROKEN WHITE PAINT (10' LINE, 30' GAP)
- 4" DOTTED WHITE PAINT (3' LINE, 9' GAP)
- 10" SOLID WHITE PAINT
- WHITE PAINT SYMBOL/LEGEND
- PORTABLE P.C.C. SAFETY BARRIER (F SHAPE)
- PORTABLE IMPACT ATTENUATOR
- ARROW PANEL
- REMOVE EXISTING STRIPING BY CONTRACTOR

**SEQUENCE OF CONSTRUCTION - PHASE 4**

UPON COMPLETION OF ALL CONSTRUCTION ACTIVITIES, REMOVE ALL PHASE 3 TEMPORARY TRAFFIC CONTROL DEVICES EXCEPT THOSE NEEDED FOR THE CLOSURE OF THE US 301 NORTHBOUND ROADWAY SOUTH OF STA. 865+00 AND THE SR 1 SOUTHBOUND RAMP TO US 301 AS SHOWN ON THIS PLAN. THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THESE PHASE 4 DRAWINGS SHALL REMAIN UNTIL OPENING OF THE US 301 ROADWAY BY OTHERS (SEE NOTES 1 & 2).

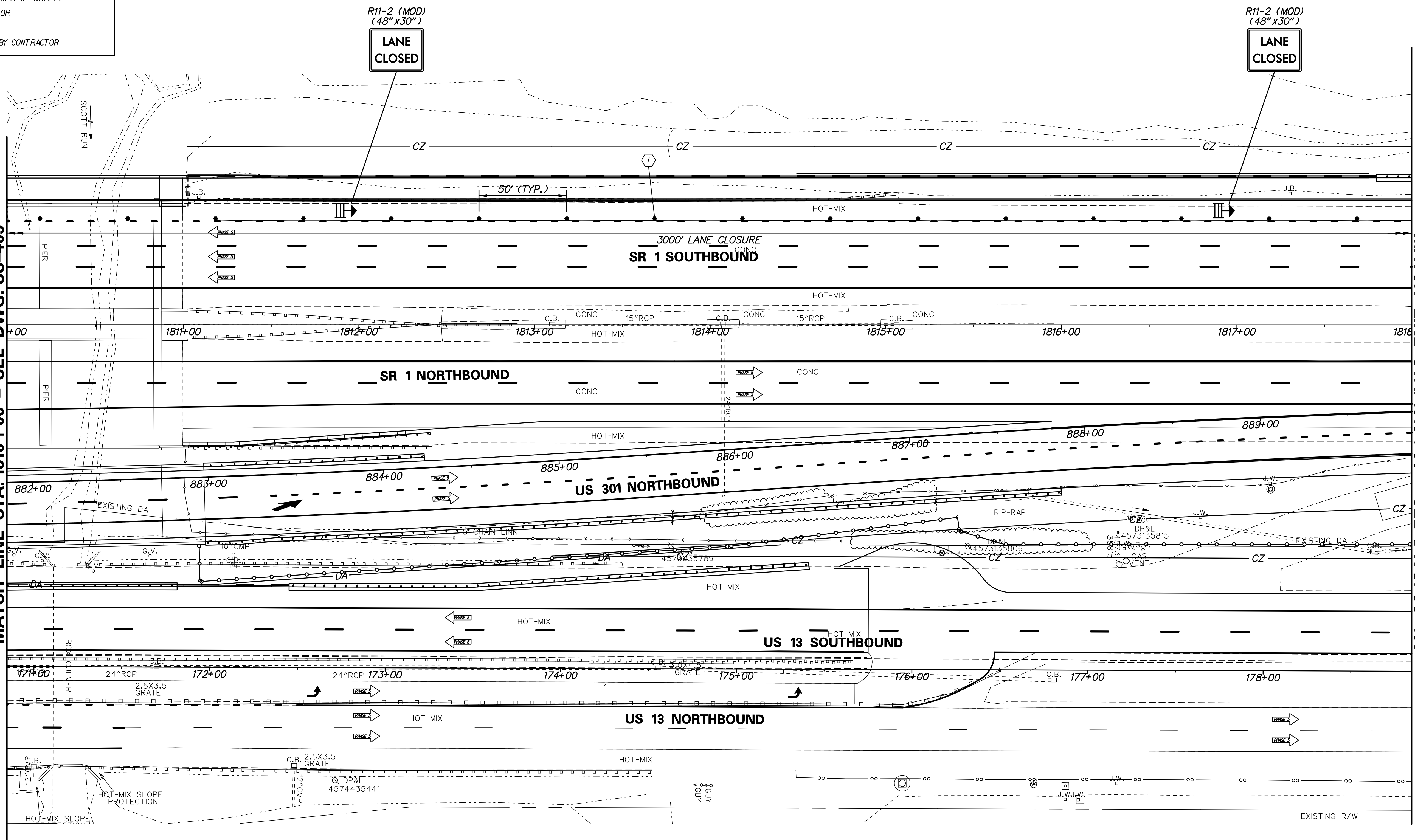
**NOTES:**

1. AT THE COMPLETION OF CONSTRUCTION THE CONTRACTOR SHALL FURNISH AND INSTALL TEMPORARY TRAFFIC CONTROL DEVICES TO REMAIN AS SHOWN ON DRAWINGS CS-401 THROUGH CS-407 (PHASE 4). THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE AND SERVE TO TEMPORARILY CLOSE THE NEW US 301 ROADWAY UNTIL ITS OFFICIAL OPENING. MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES DURING THE INTERIM CLOSURE PERIOD AND UPON OPENING OF THE US 301 ROADWAY SHALL BE THE RESPONSIBILITY OF OTHERS (CONTRACT T200911303 CONTRACTOR).
2. PAYMENT FOR MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THIS PLAN BY THE CONTRACT T200911303 CONTRACTOR WILL NOT BE MEASURED AND PAID FOR BUT WILL BE INCIDENTAL TO ITEM 743000 - MAINTENANCE OF TRAFFIC IN CONTRACT T200911303.



MATCH LINE STA. 1810+00 - SEE DWG. CS-403

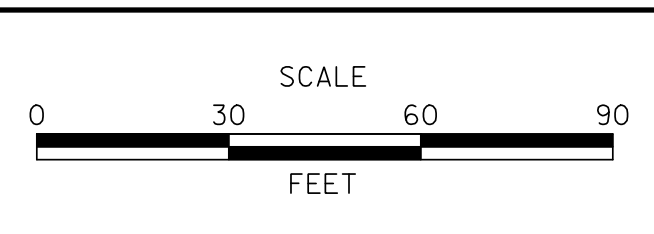
MATCH LINE STA. 1818+00 - SEE DWG. CS-405



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ADDENDUMS / REVISIONS	
REVISED ITEM NO.:	BRT/DMG



**US 301 &  
SR 1 INTERCHANGE**

CONTRACT	T200911302
COUNTY	NEW CASTLE
BRIDGE NO.	BR1-903N, BR1-903S BR1-432 & RW1-1
DESIGNED BY:	J.D.C.
CHECKED BY:	J.M.M.

**CONSTRUCTION PHASING,  
M.O.T., AND EROSION  
CONTROL PLAN - PHASE 4**

CS-404
SHEET NO.
424
TOTAL SHTS.
491

**CONSTRUCTION PHASING LEGEND**

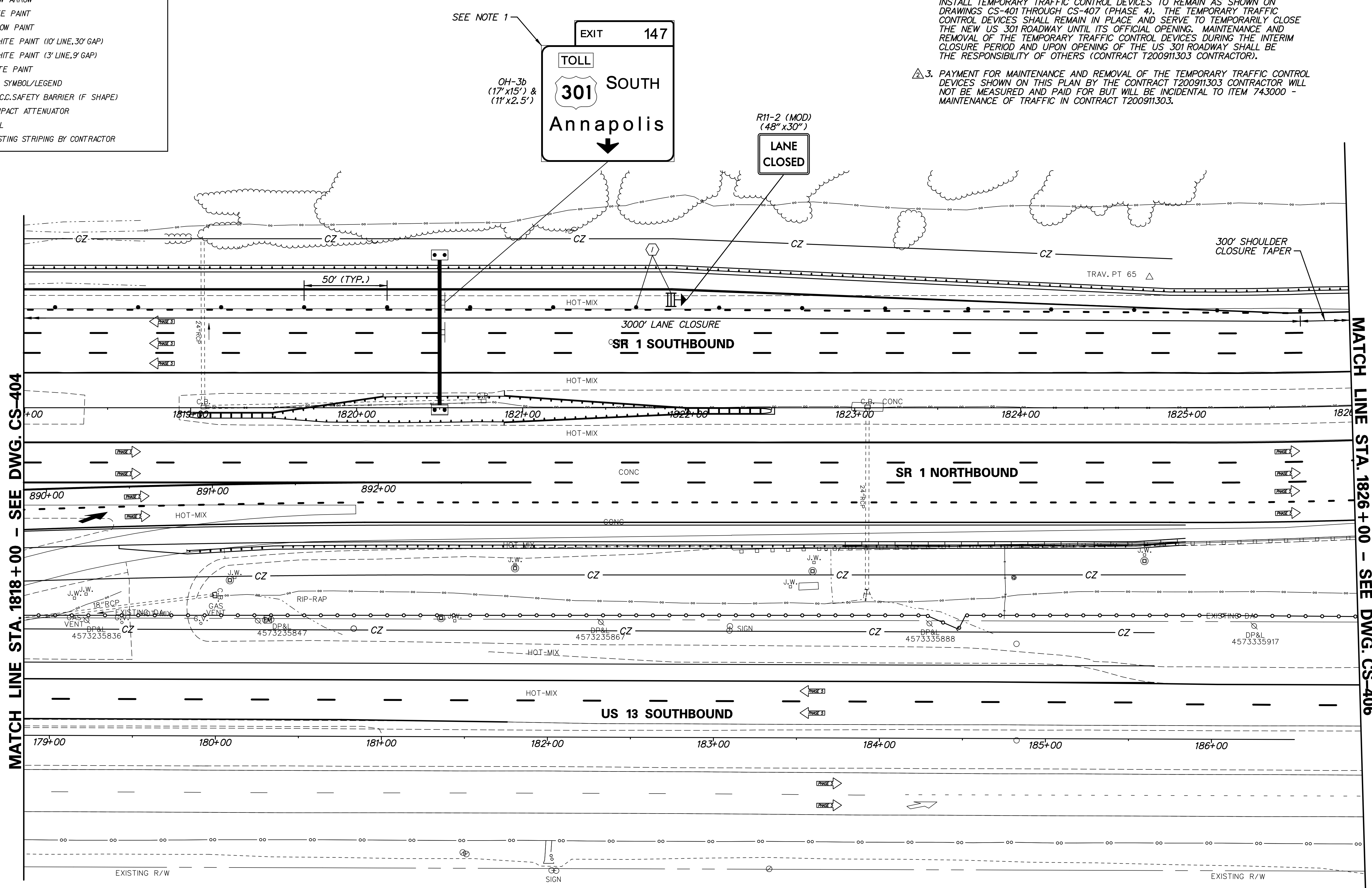
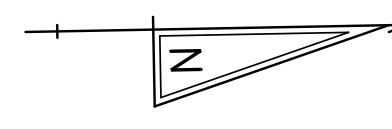
- TEMPORARY CONSTRUCTION
- PROPOSED CONSTRUCTION THIS PHASE
- EXISTING SIGN/PROPOSED TEMPORARY SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- TRAFFIC FLOW ARROW
- 4" SOLID WHITE PAINT
- 4" SOLID YELLOW PAINT
- 4" BROKEN WHITE PAINT (10' LINE, 30' GAP)
- 4" DOTTED WHITE PAINT (3' LINE, 9' GAP)
- 10" SOLID WHITE PAINT
- WHITE PAINT SYMBOL/LEGEND
- PORTABLE P.C.C. SAFETY BARRIER (F SHAPE)
- PORTABLE IMPACT ATTENUATOR
- ARROW PANEL
- REMOVE EXISTING STRIPING BY CONTRACTOR

**SEQUENCE OF CONSTRUCTION - PHASE 4**

UPON COMPLETION OF ALL CONSTRUCTION ACTIVITIES, REMOVE ALL PHASE 3 TEMPORARY TRAFFIC CONTROL DEVICES EXCEPT THOSE NEEDED FOR THE CLOSURE OF THE US 301 NORTHBOUND ROADWAY SOUTH OF STA. 865+00 AND THE SR 1 SOUTHBOUND RAMP TO US 301 AS SHOWN ON THIS PLAN. THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THESE PHASE 4 DRAWINGS SHALL REMAIN UNTIL OPENING OF THE US 301 ROADWAY BY OTHERS (SEE NOTES 1, 2 & 3).

**NOTES:**

1. CONTRACT T200911302 CONTRACTOR SHALL COVER SIGN WITH OPAQUE MATERIAL AFTER INSTALLATION OF SIGN AND LEAVE SIGN COVERED AT COMPLETION OF CONTRACT. CONTRACT T200911303 CONTRACTOR SHALL REMOVE SIGN COVERING UPON OPENING OF US 301 ROADWAY.
2. AT THE COMPLETION OF CONSTRUCTION THE CONTRACTOR SHALL FURNISH AND INSTALL TEMPORARY TRAFFIC CONTROL DEVICES TO REMAIN AS SHOWN ON DRAWINGS CS-401 THROUGH CS-407 (PHASE 4). THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE AND SERVE TO TEMPORARILY CLOSE THE NEW US 301 ROADWAY UNTIL ITS OFFICIAL OPENING. MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES DURING THE INTERIM CLOSURE PERIOD AND UPON OPENING OF THE US 301 ROADWAY SHALL BE THE RESPONSIBILITY OF OTHERS (CONTRACT T200911303 CONTRACTOR).
3. PAYMENT FOR MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THIS PLAN BY THE CONTRACT T200911303 CONTRACTOR WILL NOT BE MEASURED AND PAID FOR BUT WILL BE INCIDENTAL TO ITEM 743000 - MAINTENANCE OF TRAFFIC IN CONTRACT T200911303.



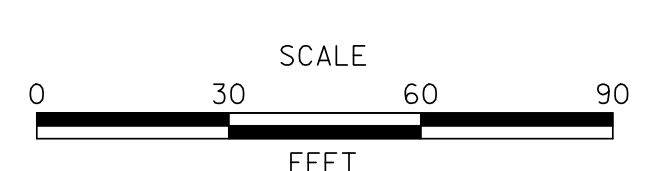
MATCH LINE STA. 1818+00 - SEE DWG. CS-404

MATCH LINE STA. 1826+00 - SEE DWG. CS-406

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ADDENDUMS / REVISIONS	
REVISED ITEM NO.:	BRT/DMG



**US 301 & SR 1 INTERCHANGE**

CONTRACT	T200911302	BRIDGE NO.	
COUNTY	NEW CASTLE	DESIGNED BY:	J.D.C.
		CHECKED BY:	J.M.M.

**CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN - PHASE 4**

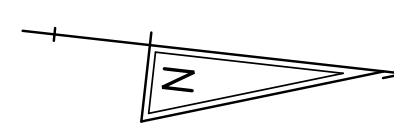
<b>CS-405</b>
SHEET NO.
425
TOTAL SHTS.
491

SEQUENCE OF CONSTRUCTION - PHASE 4

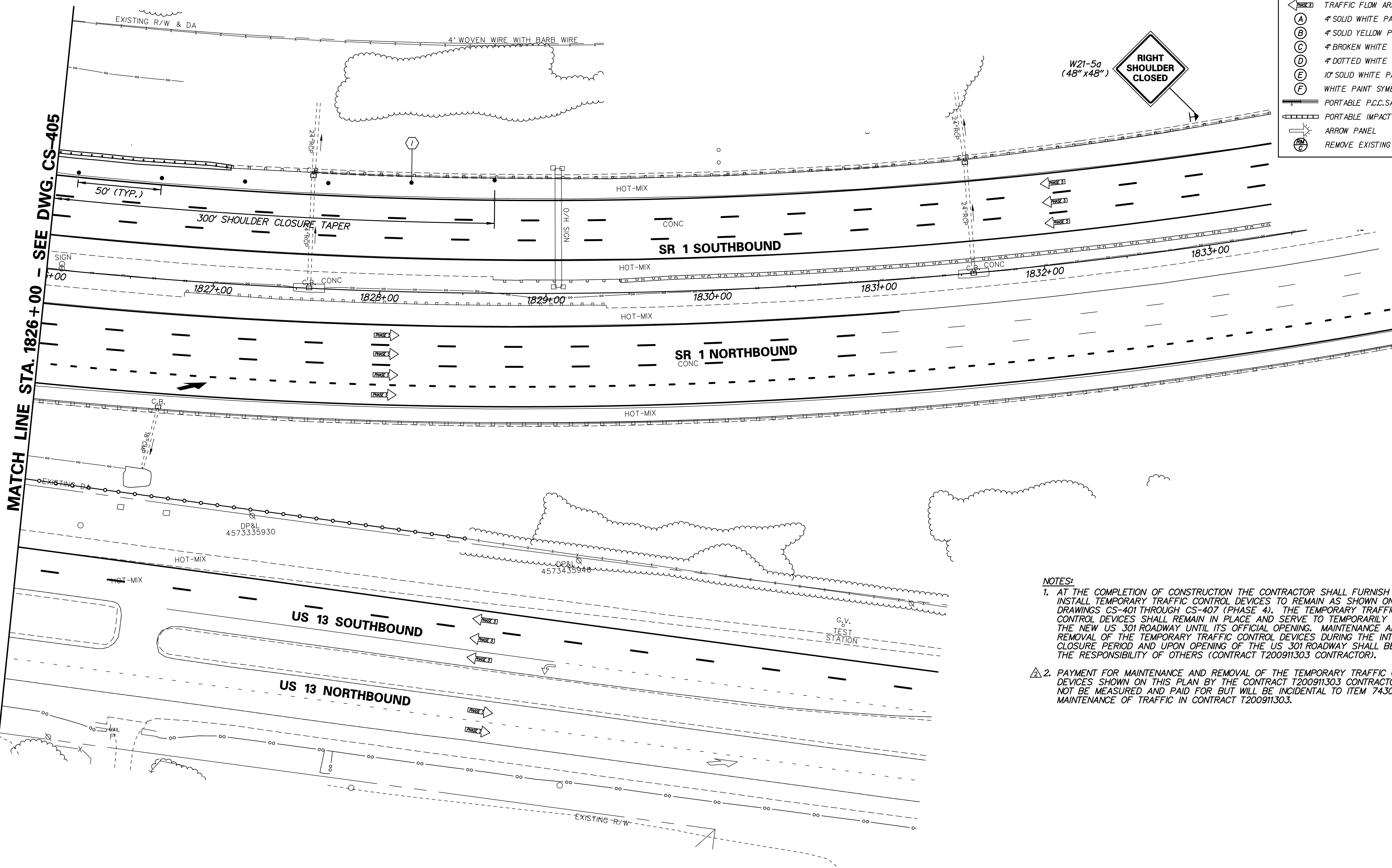
1) UPON COMPLETION OF ALL CONSTRUCTION ACTIVITIES, REMOVE ALL PHASE 3 TEMPORARY TRAFFIC CONTROL DEVICES EXCEPT THOSE NEEDED FOR THE CLOSURE OF THE US 301 NORTHBOUND ROADWAY SOUTH OF STA. 865+00 AND THE SR 1 SOUTHBOUND RAMP TO US 301 AS SHOWN ON THIS PLAN. THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THESE PHASE 4 DRAWINGS SHALL REMAIN UNTIL OPENING OF THE US 301 ROADWAY BY OTHERS (SEE NOTES 1 & 2).

CONSTRUCTION PHASING LEGEND

- TEMPORARY CONSTRUCTION
- PROPOSED CONSTRUCTION THIS PHASE
- EXISTING SIGN/PROPOSED TEMPORARY SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- TRAFFIC FLOW ARROW
- 4" SOLID WHITE PAINT
- 4" SOLID YELLOW PAINT
- 4" BROKEN WHITE PAINT (10' LINE, 30' GAP)
- 4" DOTTED WHITE PAINT (3' LINE, 9' GAP)
- 10" SOLID WHITE PAINT
- WHITE PAINT SYMBOL/LEGEND
- PORTABLE P.C.C. SAFETY BARRIER (F SHAPE)
- PORTABLE IMPACT ATTENUATOR
- ARROW PANEL
- REMOVE EXISTING STRIPING BY CONTRACTOR



W21-5a  
(48" x 48")  
**RIGHT SHOULDER CLOSED**



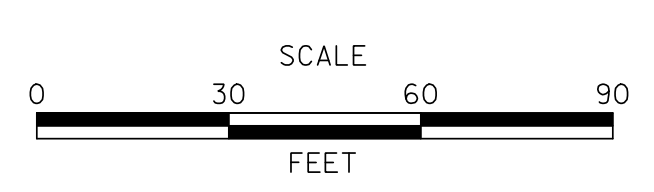
**NOTES:**

1. AT THE COMPLETION OF CONSTRUCTION THE CONTRACTOR SHALL FURNISH AND INSTALL TEMPORARY TRAFFIC CONTROL DEVICES TO REMAIN AS SHOWN ON DRAWINGS CS-401 THROUGH CS-407 (PHASE 4). THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE AND SERVE TO TEMPORARILY CLOSE THE NEW US 301 ROADWAY UNTIL ITS OFFICIAL OPENING. MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES DURING THE INTERIM CLOSURE PERIOD AND UPON OPENING OF THE US 301 ROADWAY SHALL BE THE RESPONSIBILITY OF OTHERS (CONTRACT T200911303 CONTRACTOR).
2. PAYMENT FOR MAINTENANCE AND REMOVAL OF THE TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THIS PLAN BY THE CONTRACT T200911303 CONTRACTOR WILL NOT BE MEASURED AND PAID FOR BUT WILL BE INCIDENTAL TO ITEM 743000 - MAINTENANCE OF TRAFFIC IN CONTRACT T200911303.

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ADDENDUMS / REVISIONS	
REVISED ITEM NO.:	BRT/DMG



**US 301 &  
SR 1 INTERCHANGE**

CONTRACT	BRIDGE NO.
T200911302	
COUNTY	DESIGNED BY: J.D.C.
NEW CASTLE	CHECKED BY: J.M.M.

**CONSTRUCTION PHASING,  
M.O.T., AND EROSION  
CONTROL PLAN - PHASE 4**

<b>CS-406</b>
SHEET NO.
426
TOTAL SHTS.
491